



City of North Port

ORDINANCE NO. 2025-28

AN ORDINANCE OF THE CITY OF NORTH PORT, FLORIDA, AMENDING THE CENTRAL PARC AT NORTH PORT (ACTIVITY CENTER 9) PATTERN PLAN TO REVISE THE BUFFER PLAN; AMENDING ARTICLE III, TABLE 3.3.1.2: DIMENSIONAL STANDARDS AND ARTICLE IV, DEVELOPMENT STANDARDS OF THE UNIFIED LAND DEVELOPMENT CODE TO REFERENCE THE AMENDED CENTRAL PARC AT NORTH PORT PATTERN PLAN; PROVIDING FOR FINDINGS; PROVIDING FOR CONFLICTS; PROVIDING FOR SEVERABILITY; PROVIDING FOR CODIFICATION; AND PROVIDING AN EFFECTIVE DATE.

WHEREAS, the City of North Port, Florida is committed to planning and managing the future growth and development of the City by adhering to its Comprehensive Plan and Unified Land Development Code ("ULDC"); and

WHEREAS, this Ordinance updates the Central Parc at North Port Pattern Plan by revising the Buffer Plan to better reflect current development needs, ensure consistency with the City's Comprehensive Plan, and update related provisions of the Unified Land Development Code; and

WHEREAS, these revisions are intended to enhance compatibility between land uses, improve design standards, and promote the orderly growth and development of Activity Center 9; and

WHEREAS, on July 23, 2019, the City Commission adopted Ordinance No. 2019-09, establishing the Central Parc at North Port Pattern Plan; and

WHEREAS, Applicant Kim B. Fields, Authorized Member of Manager of Sabal Trace Development Partners, LLC ("Applicant") filed Petition No. DMA-24-114, requesting an amendment to the Central Parc at North Port Pattern Plan; and

WHEREAS, on May 14, 2024, Applicant held noticed neighborhood meeting(s) regarding the proposed amendment; and

WHEREAS, the Planning and Zoning Advisory Board, designated as the local planning agency, held a properly noticed public hearing on September 18, 2025, to receive public comment on the subject matter of this ordinance and to make its recommendation to the City Commission; and

WHEREAS, the City Commission held properly-noticed public hearings at first and second reading of this ordinance, at which time all interested parties had an opportunity to appear and be heard, and the City

Commission considered all relevant evidence presented, as well as the Planning and Zoning Advisory Board's recommendation; and

WHEREAS, the City Commission finds that competent substantial evidence supports the findings and actions herein; and

WHEREAS, the City Commission finds that its action serves the public health, safety, and welfare of the citizens of the City of North Port, Florida.

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COMMISSION OF THE CITY OF NORTH PORT, FLORIDA:

SECTION 1 – FINDINGS

- 1.01 The above recitals are true and correct and are incorporated in this ordinance by reference.
- 1.02 Pursuant to Florida Statutes Section 166.041(4)(c), a business impact estimate was not required because this ordinance is enacted to implement comprehensive plan amendments and land development regulation amendments initiated by an application by a private party other than the municipality.
- 1.03 The City Commission finds the regulations in this ordinance to be consistent with the North Port Comprehensive Plan.
- 1.04 The City Commission makes the following additional findings:
 - a. This amended Pattern Plan is consistent with the ULDC, including Article IV of the ULDC, which provides the minimum requirements of the Central Parc (Activity Center 9) Activity Center Design Regulations.
- 1.05 The City Commission finds that, pursuant to Section 14 of Chapter 2023-304, Laws of Florida, this amendment to the land development regulations is not more restrictive or burdensome than current regulations.
- 1.06 This amendment is made pursuant to PMCPA-24-00000114 formerly DMA-24-114, filed by applicant Kim B. Fields, Auth. Member of Manager Sabal Trace Development Partners, LLC. The associated application was initiated by a private party, other than the City of North Port, Florida. The property that is the subject of the application is owned by the initiating private party.
- 1.07 All identified exhibits are incorporated in this ordinance by reference.

SECTION 2 – ADOPTION

- 2.01 The City Commission hereby approves and adopts the amended Central Parc at North Port Pattern Plan attached as "Exhibit A."
- 2.02 **ARTICLE III. – ACTIVITY CENTER DISTRICTS**, Table 3.3.1.2: Dimensional Standards of the Unified Land Development Code is amended to read as follows:

"ARTICLE III. – ACTIVITY CENTER DISTRICTS**Table 3.3.1.2: Dimensional Standards**

Additional restrictions apply in the Conservation Restricted Overlay District, the Myakka River Protection Zone, and adjacent to wetlands per [Chapter 6, Article III, Chapter 6, Article VI.](#) and [Chapter 6, Article VIII.](#) of this ULDC.

ZONING DISTRICT	MINIMUM BUILDING FRONTAGE (%)	SETBACKS (FT) ¹				BUILDING HEIGHT (FT)	MAXIMUM IMPERVIOUS SURFACE RATIO (%)	MAXIMUM IMPERVIOUS SURFACE RATIO (%)
		MAX. FRONT ²	SIDE ²	REAR ²	WATERFRONT ³		LOT	OVERALL ⁴
AC-1	50	25	10	10	10	100	70	70
AC-2 ⁵	65	15	10	20	10	100	70	70
AC-3	65	25	20	20	10	100	70	70
AC-4 ⁶	65	15	10	20	10	100	70	70
AC-5	65	15	10	20	10	100	70	70
AC-6	40	25	10	20	10	100	70	70
AC-7	-	-	25	-	50	40	30	30
AC-7A	50	15	10	10	20	40	70	30
AC-7B	-	-	25	40	10	40	70	30
AC-8	100	Per the West River Villages Development Master Plan DMP-22-093				70	70	70
AC-9	100	Per the Central Parc Planned Community Development Pattern Plan Exhibit C A to Ordinance 2019-09 2025-28				30 ⁷	70	70
AC-10	40	25	10	20	10	100	70	70

¹ Residential accessory structures may be located in side or rear yards only. Minimum setbacks of 10-foot rear (20-foot waterfront) and 5-foot side apply to accessory structures 300 SF or smaller. Accessory structures larger than 300 SF, other than swimming pools, screen enclosures, tennis courts and other similar recreational facilities, shall maintain the same required setbacks as the primary structure. Non-residential accessory structures may be located in any yard and must meet the primary structure setbacks.

² If the width of a required landscape buffer per [Chapter 4, Article III, Section 4.3.12](#) exceeds the required setback the buffer requirement establishes the minimum setback. If a maximum or minimum setback conflicts with an easement, the setback may be adjusted to the minimum necessary to ensure the structure does not impact access to the easement.

³ Waterfront setbacks do not apply to docks and accessory structures associated with activating the waterfront nor do they apply to platted drainage easements. See [Chapter 4, Article II, Section 4.2.7.](#)

⁴ Overall ISR applies to a development as a whole when common area is provided. Open space requirements also apply per [Chapter 4, Article IX, Section 4.9.2.](#)

⁵ Marsh Creek/Heron Creek development standards are governed by the Heron Creek Pattern Book, not the standards provided herein.

⁶ Panacea/The Woodlands and North Port Gardens development standards are governed by Development of Regional Impact Development Orders, not the standards provided above.

⁷ Maximum building height for senior living facilities is 50-feet.

”

- 2.03 ARTICLE IV. – DEVELOPMENT STANDARDS of the Unified Land Development Code is hereby amended to read as follows:

“ARTICLE IV. – DEVELOPMENT STANDARDS

...

Section 4.4.3. Activity Centers

...

B. **Activity Center 9**, Central Parc will continue to be governed by the design standards incorporated in Ordinance No. ~~2019-09-2025-28~~.

...”

SECTION 3 – CONFLICTS

- 3.01 In the event of any conflicts between the provision of this ordinance and any other ordinance, in whole or in part, the provisions of this ordinance shall prevail to the extent of the conflict.

SECTION 4 – SEVERABILITY

- 4.01 If a court of competent jurisdiction finds that any section, subsection, sentence, clause, phrase, or provision of this ordinance is for any reason invalid or unconstitutional, that provision will be deemed a separate, distinct, and independent provision and will not affect the validity of the remaining portions of the ordinance.

SECTION 5 – CODIFICATION

- 5.01 In this ordinance, additions are shown as underlined and deletions as ~~striketrough~~. Any additional codification information and notations appear in *italics*. These editorial notations shall not appear in the codified text.

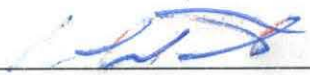
SECTION 6 – EFFECTIVE DATE

- 6.01 This ordinance shall take effect immediately upon adoption.

READ BY TITLE ONLY at first reading by the City Commission of the City of North Port, Florida in public session on October 14, 2025.

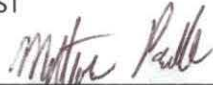
ADOPTED by the City Commission of the City of North Port, Florida on the second and final reading in public session on October 28, 2025.

CITY OF NORTH PORT, FLORIDA



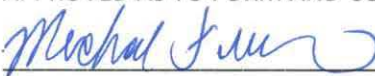
PHIL STOKES
MAYOR

ATTEST



for HEATHER FAUST, MMC
CITY CLERK

APPROVED AS TO FORM AND CORRECTNESS



MICHAEL FUINO, B.C.S.
CITY ATTORNEY

Central Parc

AT NORTH PORT



Planned Community Development (PCD) Pattern Plan

Original Adoption: July 23, 2019, Ordinance No. 2019-09

Kimley»Horn

Amended October 28, 2025, Ordinance No. 2025-28

Prepared by:

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Kimley»Horn

Environmental consulting provided by:

IVA Environmental Consulting Services
4050 Rock Creek Drive
Port Charlotte, Florida 33948
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Central Parc at North Port at North Port Planned Community
Development (PCD) Pattern Plan

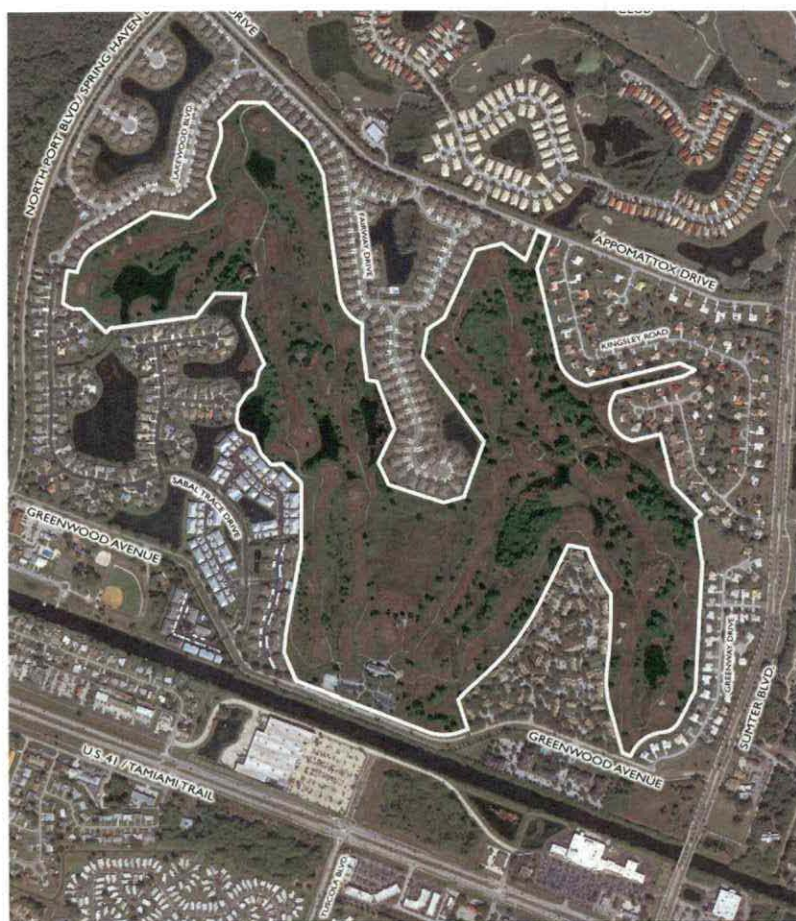
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01

Project Overview + General Description

Central Parc at North Port Planned Community Development (PCD) Pattern Plan



The area known as Central Parc at North Port Planned Community Development (PCD) is located north of Greenwood Avenue and south of Appomattox Drive between North Port Boulevard and Sumter Boulevard. The property was previously the subject of a masterplan approval under its future land use designation and zoning consisting of 614 multi-family units in multi-story buildings, some up to four (4) stories in height. The approved development plan proposed to redevelop the existing Sabal Trace golf course into a replica style course with accompanying new residential development and additional commercial development. That plan approval included a Development Concept Plan (DCP) and Preliminary Subdivision Plan (PSP). On January 9, 2006 DCP 05-84 and PSP 05-85 were approved by the City of North Port. Due in large part to the Great Recession, the approved project was not developed and the approvals have since expired.

The property was originally zoned Residential Multi-Family (RMF) with a future land use designation of residential. In 2001, Charlotte Golf Partners bought the property envisioning 800 homes ancillary to the golf course. However, without Charlotte Golf Partners' approval, the City initiated a rezoning from RMF to Commercial Recreation and changed the future land use designation from residential to Recreation Open Space, resulting in litigation and an eventual Settlement Agreement.

The property's residential character will now be restored, while providing a logical transition from Activity Center #1 with new mixed-use development areas, and new residential uses. The proposed plan is similar in nature to the previously approved project, as it proposes infill development taking advantage of all existing City infrastructure and a location proximate to retail development, schools, and a major arterial roadway. The proposed plan anticipates a main entry from Greenwood Avenue with secondary and emergency

access to Appomattox Drive. The main change to the plan is removal of all golf course facilities, rather focusing on exterior landscape buffering, water features, and open spaces. The current plan anticipates a mix of predominantly residential single-family detached and attached villa style units with two (2) parcels that will incorporate a mixture of uses.

The mixed - use development areas will be adjacent to Greenwood Avenue and be accessed from the main entry road. The plan for the mixed use development areas may include medical office, ancillary club house, senior living facility, retail, storage facilities for the development and adjacent development, neighborhood commercial, and multi-family. The typical single-family and villa type residential units proposed solely within the residential development areas will not exceed two (2) stories and the style will be compatible with the surrounding developed neighborhoods with single-family and villa type units.

The following Pattern Plan is designed in general conformance with the North Port Zoning Regulations and the City's Urban Design Standards Pattern Book. The following Pattern Plan is for Central Parc at North Port Planned Community Development (PCD) adopted by the City of North Port on ____.

Central Parc at North Port Planned Community Development (PCD) Pattern Plan



Concept Plan

Figure 1

02 Mixed Use Development Areas

Central Parc at North Port Planned Community Development (PCD) Pattern Plan



The Mixed Use Development Areas will be adjacent to Greenwood Avenue and accessed from the main entry road. The plan for the mixed-use area may include medical office, ancillary club house, senior living facility, retail, storage facilities for the new and existing adjacent development, neighborhood commercial, and multi-family.



Through planning and economic development efforts, creating interconnectivity and options for transportation, along with the continued development of Activity Centers, the City's goal is to provide the opportunity to create a live/work community, which would decrease much of the need to use I-75, and increase the overall quality of life for residents. This includes not only the live/work concept, but also includes issues such as connectivity between neighborhoods, schools, civic areas, and commercial areas.



The original core of the City is Activity Center #1 (the US-41 corridor, south of the Subject Property) and the adjacent residential areas. The 1997 Comprehensive Plan identified this area and after this Comprehensive Plan, the City made it a priority to enhance the attractiveness of the area and to create a positive environment for the location of new businesses through redevelopment.

All development within the two Mixed Use Development Areas of Central Parc at North Port adheres to the "Architectural Guidelines" for the AC #9 and the Urban Design Standards Pattern Book (UDSPB) pursuant to Section 55-60 et seq., ULDC to create a specific identifying character, continuity, and connectivity within the Activity Center. It is generally intended to utilize these Activity Center regulations to implement the Comprehensive Plan, Future Land Use Element, Goal 2, Policy 2.9.1: This Activity Center shall be established to provide infill development with a mixture of low and medium density residential uses, as well as senior living, retail storage, medical office, and neighborhood commercial to serve the surrounding neighborhoods and interface with residential and commercial along the US-41 corridor.

Residential Development Areas

The proposed development includes a mixture of housing types, sizes, and prices in a location adjoining existing and proposed workplaces, commercial uses, and office uses to benefit current and future residents of North Port. All residential development within Phases 1 and 2 of Central Parc at North Port will adhere to the architectural guidelines in this Central Parc at North Port PCD Pattern Plan to create a specific identifying character, continuity, and connectivity.

03 Land Use Standards

Central Parc at North Port Planned Community Development (PCD) Pattern Plan

Figure 2	Mixed Use Development Areas					Residential Development Areas	
	Medical Office	Multi-Family		Commercial		Single-Family Detached	Single-Family Villas
		Senior Living	Conventional Multi-Family	Neighborhood Commercial	Mini Storage (4)		
Maximum density/intensity	0.50 FAR	300 beds / 0.50 FAR	10 du/acre	0.5 FAR	35% of available lot area	4 du/acre	10 du/acre
Minimum lot requirements	Minimum land area of 7,500 SF with a minimum width of 50-feet measured along the front property line	Minimum land area of 7,500 SF with a minimum width of 50-feet measured along the front property line	Min. lot size: 8,700 SF	Minimum land area of 7,500 SF with a minimum width of 50-feet measured along the front property line	Minimum land area of $\geq \frac{1}{2}$ acre and have a minimum frontage of 100-feet	See dimensional and performance standards	See dimensional and performance standards
Maximum lot coverage	50%	50%	50%	50%	50%	50%	50%
Minimum dwelling living area	--	350 SF	1,000 SF	--	--	1,500 SF	1,000 SF
Maximum building size	30,000	--	--	20,000	80,000SF	--	--
Minimum setback requirements(1)	--	--	--	--	--	See dimensional and performance standards for setbacks	See dimensional and performance standards for setbacks
Front Yard:	0-feet	10-feet	10-feet	0-feet	10-feet	--	--
Side Yard:							

In general, Central Parc at North Port PCD is divided into two types of development area. These development areas are designated as Mixed Use or Residential. Land uses within the Mixed-Use and Residential Development Areas shall be regulated in part by Figure 2. The following figure shall serve as the allowable land use regulating instrument for permitting development within the Mixed-Use and Residential Development Areas. Refer to Figure 2 of this Pattern Plan for locations of the individual development areas.

(1) For single-family residential uses see Dimensional and Performance Standards (Page 8)

(4) Mini-storage development shall not exceed 7 acres of land area.

Central Parc at North Port Planned Community Development (PCD) Pattern Plan

Figure 2	Mixed Use Development Areas					Residential Development Areas	
	Medical Office	Multi-Family		Commercial		Single-Family Detached	Single-Family Villas
		Senior Living	Conventional Multi-family	Neighborhood Commercial	Mini Storage (4)		
Interior:	0-feet	10-feet	10-feet	0-feet	None unless a landscaping buffer is required as set forth elsewhere in the ULDC	-- --	-- --
Abutting a road:	0-feet	10-feet	20-feet	0-feet	15-feet	-- --	-- --
Abutting residential zoning:	30-feet	30-feet	20-feet	30-feet	30-feet	-- --	-- --
Rear yard:	20-feet	20-feet	20-feet	20-feet	20-feet	-- --	-- --
Abutting a road:	0-feet	10-feet	20-feet	0-feet	15-feet	-- --	-- --
Abutting water:	25-feet	25-feet	25-feet	25-feet	25-feet	-- --	-- --
Waterfront yard or drainage right-of-way:	25-feet	25-feet	25-feet	25-feet	25-feet	-- --	-- --
Maximum building height	35-feet	50-feet	35-feet	35-feet	35-feet	35-feet	35-feet
Minimum open space (2) (3)	10%	15%	15%	10%	10%	25%	25%

(2) Open Space percentage is on-site only. Total project open space for Central Parc at North Port is 30% minimum.

(3) Each individual infrastructure / subdivision / site plan application must provide and open space tracking chart to verify minimum open space requirements are being met for the entire site.

(4) Mini-storage development shall not exceed 7 acres of land area.

Land Use Standards

Central Parc at North Port Planned Community Development (PCD) Pattern Plan

Figure 2	Mixed Use Development Areas					Residential Development Areas		Prohibited Uses & Structures
	Medical Office	Multi-Family		Commercial		Single-Family Detached	Single-Family Villas	
		Senior Living	Conventional Multi-family	Neighborhood Commercial	Mini Storage			
Permitted Uses	(a) Adult living facilities. (b) Art galleries, museum. (c) Banks and other financial institutions (no drive-through). (d) Business services. (e) Emergency and essential services. (f) Exercise gymnasium. (g) Houses of worship, provided that minimum parcel size shall not be less than two (2) acres. (h) Low and Medium- Density residential. (i) Professional office including medical and dental office / clinic. (j) Mini-storage facility. (k) Personal services. (l) Post Office. (m) Professional services. (n) Recreational or community center maintained by a Community Development District or a homeowner's association, as defined in F.S. 720.301 (9) (o) Restaurant (no drive-through)					(a) Family day care. (b) Model home. See Sec. 53-240 (U) of the Unified Land Development Code. (c) Non-profit park or playground. (d) PODS shall be permitted in accordance with Sec. 53-265 (A)(2) of the Unified Land Development Code. (e) Recreational or community center maintained by a Community Development District or a homeowner's association, as defined in F.S. 720.301(9). (f) Single-family residential. (g) Two-family dwelling.		(1) Adult exhibition / entertainment. (2) Adult gaming / casino. (3) Bail bondsman. (4) Billboards. (5) Cemetery. (6) Check quick cashing and quick loan establishments. (7) Equipment rental. (8) Fireworks sales or manufacturing. (9) Funeral home. (10) Heavy machinery and equipment sales and service. (11) Incinerator. (12) Junkyard or automobile wrecking yard. (13) Manufacturing or industrial establishments. (14) Pawn shop. (15) Restaurant, drive-through. (16) Tattoo parlor or body art establishment. (17) Truck stop. (18) Warehouse and storage, except as permitted in Sec. 55-61. (19) Wholesale establishment. (20) Wholesale club. (21) The parking and storage of construction equipment in connection with authorized active, continuing construction on the premises. (22) The keeping of farm animals, exotic animals, livestock and poultry, or any animal normally found in the wild. (23) Any car service activity other than that done by the homeowner, or by a service professional who has been granted permission by the homeowner, to the homeowner's vehicle shall be expressly prohibited. (24) All communication towers including but not limited to television and radio transmitter towers, are prohibited. This section is not meant to prohibit small- cell deployment.

04 Dimensional + Performance Standards

Central Parc at North Port Planned Community
Development (PCD) Pattern Plan

Residential development within is described as one of the following structure types: Single-Family Detached - Type A, Single-Family Detached - Type B, Single-Family Attached Villas. These residential structure types are unique in their configuration and are sited on specific lot sizes with corresponding setback standards. Typical of the structure types are shown in Figures 3 through 5. Figures 6, 7, and 8 graphically describe each residential structure type while establishing dimensional standards for the said structures.

single-family type a detached



Figure 3

single-family type b detached



Figure 4

single-family attached



Figure 5

Central Parc at North Port Planned Community Development (PCD) Pattern Plan



FIGURE 6 - SINGLE-FAMILY DETACHED - TYPE A

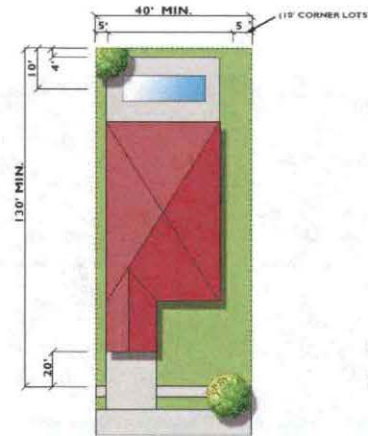


FIGURE 7 - SINGLE-FAMILY DETACHED - TYPE B

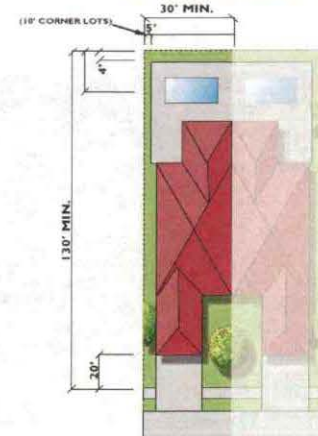


FIGURE 8 - SINGLE-FAMILY ATTACHED

DEVELOPMENT STANDARDS	SINGLE-FAMILY DETACHED - TYPE A	SINGLE-FAMILY DETACHED - TYPE B	SINGLE-FAMILY ATTACHED
LOT AREA (MIN)(8)	6,500 SF	5,200 SF	4,160 SF (per unit)
LOT WIDTH(MIN)(7)	50 FT	40 FT	32 FT
LOT DEPTH(MIN)	130 FT	130 FT	125 FT
LOT COVERAGE (MAX)(8)	50 %	50 %	50 %
FRONT SETBACK (MIN) (6)	20 FT (FLG) / 14 FT (SLG)	20 FT (FLG) / 14 FT (SLG)	20 FT / 14 FT (SLG)
SIDE SETBACK (MIN) (STRUCTURES)(3)(4)(6)	5 FT 10 FT (CORNER LOTS)	5 FT 10 FT (CORNER LOT)	5 FT 0 FT (COMMON WALL or SHARED LOT LINE) 10 FT (CORNER LOT)
SIDE SETBACK(MIN) (POOL DECKS, PATIOS, AND SCREEN ENCLOSURES)(2)(3)(6)(8)	3 FT	3 FT	4 FT (WITH SIDE YARD) / 0 FT (COMMON WALL / SHARED LOT LINE) (POOL EDGES HAVE A 5 FT SETBACK ON SIDE YARDS OF UNITS)
REAR SETBACK (MIN) (5)(6)	10 FT (STRUCTURE) / 4 FT (DECK/PATIO) / 5 FT (POOL EDGE)	10 FT (STRUCTURE) / 4 FT (DECK/PATIO) / 5 FT (POOL EDGE)	10 FT (STRUCTURE) / 4 FT (DECK/PATIO) / 5 FT (POOL EDGE)

NOTES: To be able to adjust to marketing conditions, changes to the Building Code, resident input, etc. product styles / building footprints illustrated in the figures above may be adjusted from shown and shall meet all dimensional standards.

(1) Front loading garage models (FLG) shall have a larger front setback than side loading garage models (SLG).

(2) Screen enclosures for Single-Family Attached will have a 5 FT side setback without a privacy wall, or a 0 FT side setback with a privacy wall, provided that the screen is located atop the privacy wall.

(3) Patios and pool decks for Single-Family Attached may have a 0 FT side setback provided they abut a shared privacy wall.

(4) Corner setbacks do not apply when the side property line is adjacent to a platted open space tract of at least 10 FT in width.

(5) The rear setback for pools, pool decks, and screen enclosures may be reduced to 0 FT when the rear property line abuts an easement, water body, or open space tract of at least 50 FT in width.

(6) Cornices, veneers, or other non-structural projections shall not count towards setbacks. They shall be treated similar to roof overhangs.

(7) Min. lot area and width for curvilinear lots may be less than required provided that all min. setback requirements are met and the average lot width (front lot line and rear lot line) is equal to or greater than the min. lot width required.

(8) Lot Coverage is defined as percent of lot area under fixed roof. Lot Coverage does not include pools, decks, driveways, patios, sidewalks, etc.

Dimensional + Performance Standards

Central Parc at North Port Planned Community
Development (PCD) Pattern Plan

The architectural intent of mini storage structure types are shown in Figures 9 through 12. The intent shall match the same architectural style as the AC #9 for mixed use.

Main Components are:

- Single Story (except architectural features such as a tower).
- Exterior walls shall have architectural features such as relief, banding, reveals, cornices, etc. and walls shall incorporate change in plane.
- Roof lines along the exterior shall be hip or gable style and finishes to custom with AC #9 standards.



Figure 9



Figure 10



Figure 11



Figure 12

05 Roadway Plan

Central Parc at North Port Planned Community Development (PCD) Pattern Plan



Central Parc at North Port PCD design allows for the following primary street types: Boulevard Streets (Entrance Road only), and Type I Residential Local Roads (Residential Development Areas only).

Central Parc at North Port Planned Community Development (PCD) Pattern Plan

Enhanced buffering, including lakes and swales, that exceeds the basic code requirements is proposed between the two (2) Mixed Use Development Areas and the new residential development, as well as the new residential development and the existing residential development.

(1) Lane Width Includes 2-foot Wide Concrete Curb

*Provide 8' Sidewalk where entry road connects to Appomatox Drive.

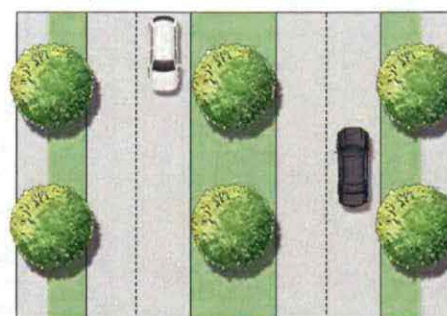
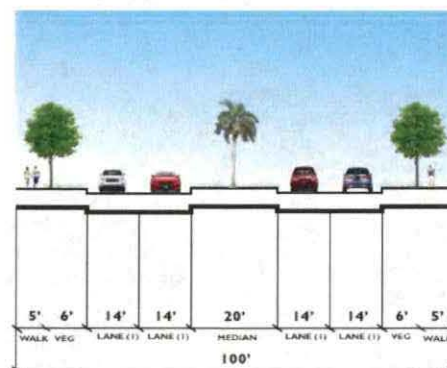


Figure 13
Typical Boulevard Road Section + Plan

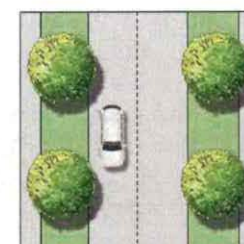
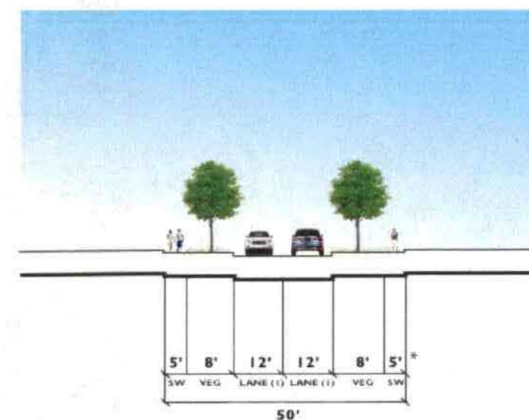


Figure 14
Typical Residential Local Road Section + Plan

Roadway + Buffer Plan

06

Emergency Vehicle Access Plan

Central Parc at North Port Planned Community
Development (PCD) Pattern Plan

Emergency access to Central Parc at North Port PCD will be provided during the interim phases of construction until build out of the project occurs. Emergency access will be provided by the Greenwood Avenue main entrance as well as the secondary entrance off Appomattox Drive.

Central Parc at North Port Planned Community Development (PCD) Pattern Plan



Note:
Mixed Use #1 and #2 will be developed under separate MAS applications. The exterior roadway and utility infrastructure to support Mixed Use #1 and #2 will be completed with Phase 1.

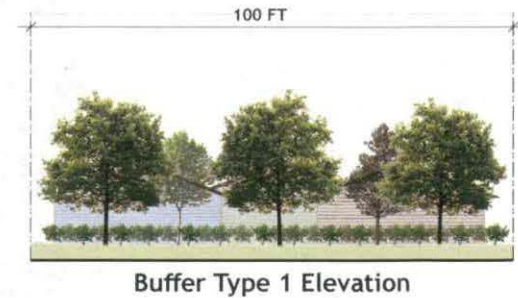
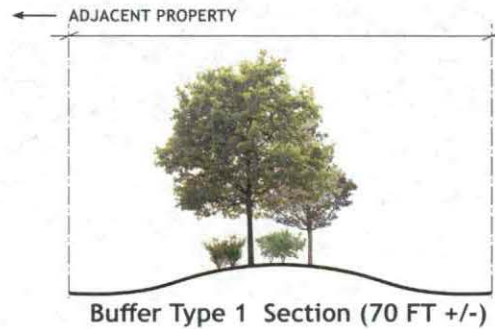
Phasing Plan

07 AMENDED Buffer Plan

Central Parc at North Port Planned Community Development (PCD) Pattern Plan

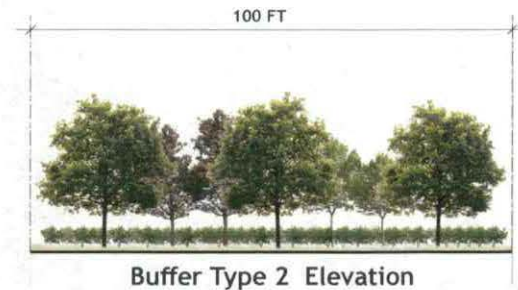
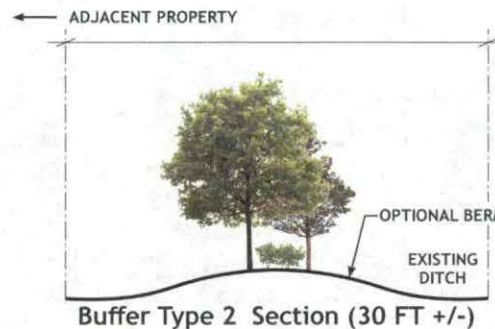
Buffer Type 1 (Typical 100 FT)

- 3 Canopy Trees
- 3 Understory Trees
- 30 Shrubs (can be grouped together)
- 3 FT to 5 FT high berm (Maximum 4:1 (H:V) side slopes)



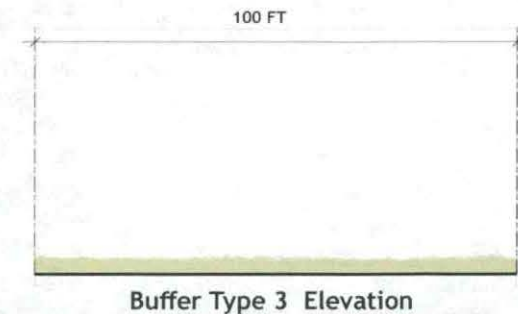
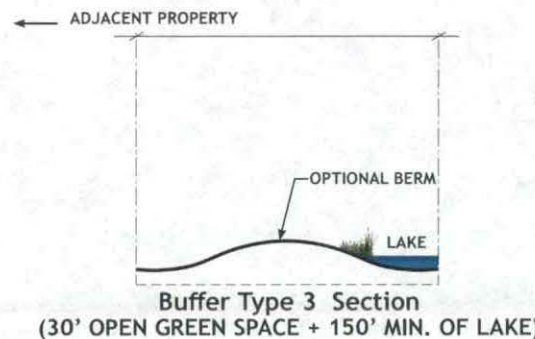
Buffer Type 2 (Typical 100 FT)

- 3 Canopy Trees
- 3 Understory Trees
- 30 Shrubs (can be grouped together)
- Optional 1FT to 3FT high berm (Maximum 4:1 (H:V) side slopes)



Buffer Type 3 (Typical 100 FT)

- Sod
- Optional 1FT to 3FT High Berm (Maximum 4:1 (H:V) side slopes)



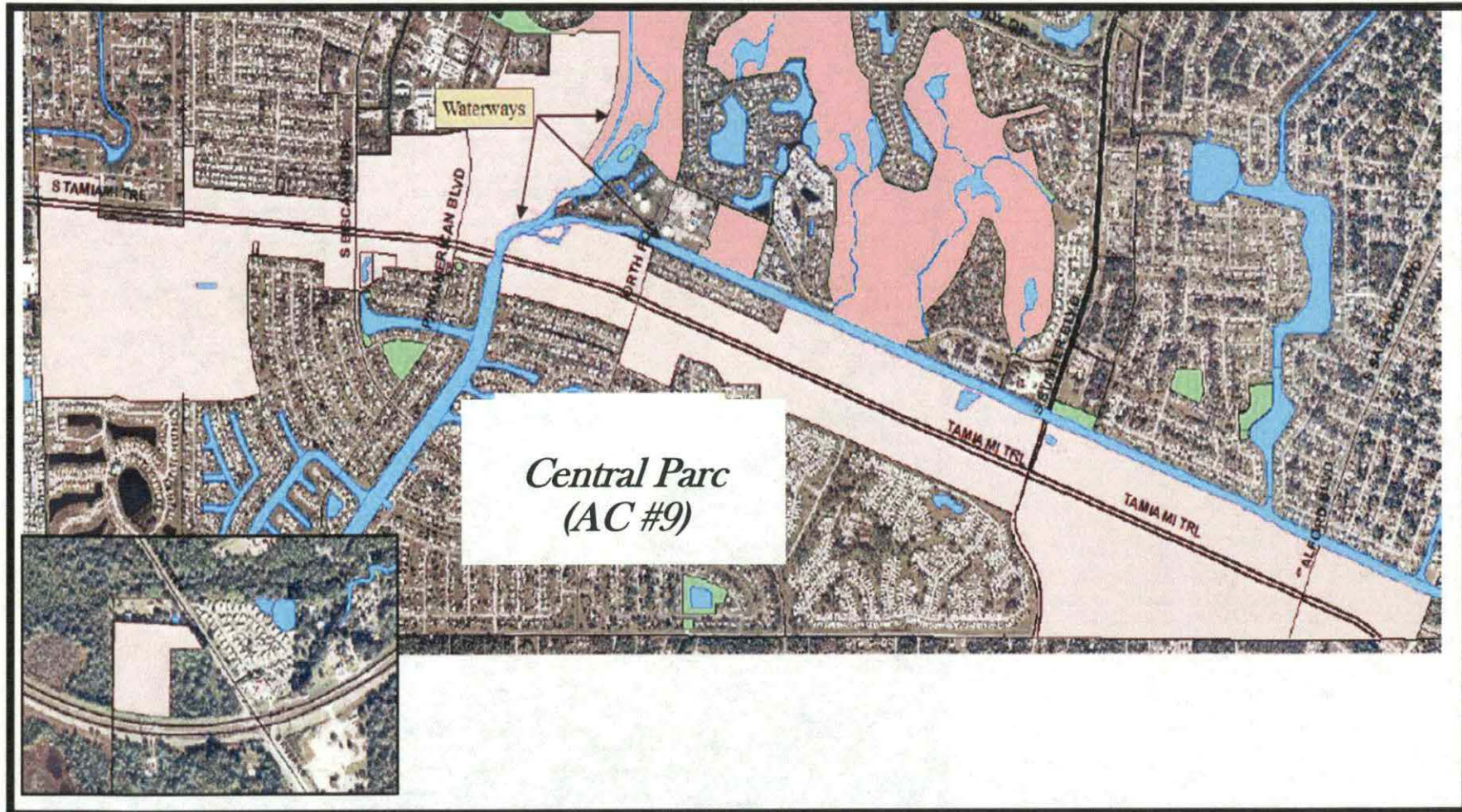
07**AMENDED Buffer Plan**Central Parc at North Port Planned Community
Development (PCD) Pattern Plan**FOOTNOTES**

1. BUFFER 2 ADJACENT TO LAKE 9 CONSISTS OF THE EXISTING 10 OAK TREES AND SHRUBS PLANTINGS PER THE TYPICAL SECTION.

2. INSTALL 200 LF OF BUFFER 2 ALONG THE REAR OF LAKE 13 BEHIND 5435, 5441, 5445, AND 5449 APPOMATTOX DRIVE.

CENTRAL PARC

Activity Center #9



All areas identified shall be designated Central Parc (Activity Center #9), present and future, and shall be regulated by these Urban Design Standards, the Unified Land Development Code, and any approved Development Master or Concept Plan in effect. All roadway cuts that intersect with public rights of way shall be approved by the City of North Port.

Style and Design

CHART #1

Name	General Location	Architectural Style (1)
Activity Center #9/Central Parc	Greenwood Avenue	Mediterranean (Mixed Use Development Areas)
All Activity Centers		Clock Towers: Locate within public R-O-W on arterial and collector streets. Fountains: to be located in a public space incorporated into the design.

CHART #2

Colors ⁽¹⁾

Name	General Location	Private Color	Public Color
Activity Center #9/Central Parc	Greenwood Avenue	Terra Cotta (Pantone 1605 C) (Mixed Use Development Areas)	North Port City Center Green
All Activity Centers		N/A	All metal clock towers (black, white or Private Activity Center color)

(1) All colors shall be on the dark end of the spectrum and shall be approved by the City. Bright, florescent or neon colors are prohibited.

(2) North Port City Center Green is Pantone 567C.

Style and Design

CHART #3

Site Amenities, Part I

Commercial Center Designation/Name	Street Furniture (1)	Lighting	Brick Paving (2)	Street Trees (3)	Canals (4)	Buried Utilities (On-site)	Public Art
Activity Center #9/Central Parc	Y	Y	Y	Y	Y	Y	Y

- (1) Street furniture to include: benches, planters, bollards, smoking urns, trash and recycling receptacles, water fountains (pet friendly), and clock towers.
 (2) All pedestrian crosswalks, sidewalks, and ADA curb cuts shall be in the design specified for each Activity Center. Stamped concrete may be used in lieu of brick pavers, subject to City approval.
 (3) Consistent with the Unified Land Development Code, Chapter 21 & 45.
 (4) Canal treatments (Blueway Commons) shall be applicable to Activity Centers only. Future Activity Centers or lands rezoned to Planned Community Development (PCD) that include major canals will be required to include the design standards outlined in this document.

CHART #4

Site Amenities, Part II

Commercial Center Designation/Name	Walking/Fitness Path along Blueway (w/Pet-Friendly Water Fountain) (1)	Gazebo/Pavilion	Bike Racks	Clock Tower	Wet Stormwater w/ Fountain & lighting (2)	Transit Shelters (3)
Activity Center #9/Central Parc	Y	Y	Y	Y	Y	Y

- (1) All walking/fitness trails shall be of a City approved surface.
 (2) If wet ponds are located in the front or side of buildings, they shall include a fountain, waterfall or similar aeration device with accent lighting and used as a pedestrian amenity. Retention is required for signature locations, and when stormwater requirements exceed thresholds established in the Unified Land Development Code. Pump size and spray pattern to be determined based on individual site design. Stormwater systems may be located underground with a plan approved by the City's Planning, Zoning and Engineering Department.
 (3) All transit shelters shall be North Port City Center Green.

CENTRAL PARC

Activity Center #9

This Activity Center was established to provide for retail, residential, office, and commercial uses. It is located north of US 41 along Greenwood Avenue. All steel and aluminum shall be painted, or anodized, all wood shall be painted or stained, ornament and trim colors shall be compatible and harmonious with the building colors. High intensity bright and metallic colors are prohibited. Private colors within a development shall be "Terra Cotta" as indicated in the chart on page 6. The color of all outside furniture and metal located in this activity center shall be "North Port City Center Green".

Permitted uses for Central Parc are listed in Chapter 55 of the Unified Land Development Code. All development within the Mixed Use Development Areas in Central Parc shall provide the design elements in a similar style to those listed and shown:

Architectural Style: Mediterranean

Private Color: Terra Cotta (Pantone 1605C)

Public Color: North Port City Center Green (Pantone 567C)



Benches – shall be made of metal with a curved back with similar embellished design as shown. The bench back and seat shall be connected with gooseneck armrests.



Bike Racks - at least one bike rack shall be provided per development site and shall be rounded corner rectangular as shown. Slots for 4 bikes shall constitute one bike rack as required by the Unified land Development Code. Calculations for the number of bike racks per site may be found in the City's ULDC.



Brick pavers or stamped concrete shall be used for all pedestrian crossings both internal and external to the site and shall be in a herringbone pattern. The design shall be red brick bordered by concrete bands as shown. The minimum width shall be 5 feet. All pedestrian crossings on collector or arterial roadways shall be City approved design as used on Sumter and Toledo Blade.



Building height shall be a maximum of 35 feet. (Except that senior living facilities shall have a maximum height of 50 feet). Enhanced façade and landscaping is required when development abuts a waterway, residential zoning, park lands, or R-O-W.

Dumpsters areas shall be built to Public Works specifications, shielded from public view and screened with landscape plantings.

Minimum Size: the corral pad must be constructed of concrete four inches thick, inclined from front to back to allow water run off (one inch in twelve feet) with minimum interior dimensions of the corral twelve by fourteen feet exclusive of any posts or bumpers. The length of the largest dumpsters is ten feet. That requires a minimum of 12 feet from any interior post or bumper to the closed gate. Tenants can then gain 360E access to the dumpster to fill it equally.

The Approach: the approach to the pad must be a hard surface to support the weight of a 25,000+ pound truck. The approach elevation must be nearly level to allow alignment and meshing of the truck and the dumpster. Ideally, a turnaround area for the truck would be provided so the final approach to the pad to be straight for at least 30 feet. This would also prevent excess moving of the truck that can damage non- concrete road surfaces.

The Corral: a solid masonry wall will enclose the dumpster pad, minimum six feet high on three sides, with the gate at the front. The front gate must be a double gate, the width of the corral frontal area, minus required structural supports on the sides. The front gate shall have no center gate support to impede access to the dumpster. The most successful designs incorporate anchor or hurricane fence gates regardless of the side material. This gate design would allow the truck an unobstructed approach. The entire corral, including a gate, must be a material that is opaque.

Fences shall be consistent with the building design. Stained wood and coated chain link fences may be used in areas not visible from the street and black decorative wrought iron fences may be used in approved areas. Split rail, stockade or similar type fencing is prohibited.

Landscape designs shall promote the reduction of future maintenance. It is the developer's responsibility to maintain all landscaping.

Lighting – light poles shall be as shown in figure #1 and shall be painted North Port City Center Green within the development. Lights are to be placed within the development along all roadways, drives and footpaths. If connector or major local roadways are designed as part of a development, street lights shall be as shown in figure #2. All pedestrian areas shall maintain a minimum of 0.9 foot candles.



Figure #1



Figure #2

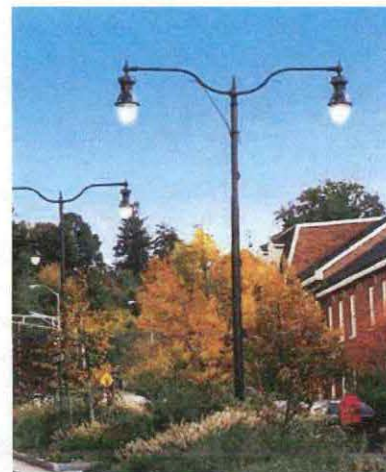


Figure #3



Figure #2

Outdoor displays are allowed with a Special Exception permit. All displays must be harmonious with the architectural design and shall not impose a pedestrian or traffic hazard. No display shall block pedestrian passage along any sidewalk.

Outdoor sales and storage are prohibited except as provided. All storage shall be within a fully enclosed building. Outdoor vehicle and boat storage as an accessory use to a mini storage facility shall be underneath a fixed roof that meets the design requirements of this Activity Center.

Outdoor seating areas shall use the courtyard approach and shall use a courtyard setting with canopy trees as shown. If all setbacks, site lines and safety regulations are followed, the use of a widened sidewalk may be used to accommodate a seating area.



Parking requirements are found in Chapter 25 of the City's ULDC.

Planters shall be placed along sidewalks and entryways in the Mixed Use Development Areas. Planters shall be made of concrete or of a material with similar appearance and shall be terra cotta in color. The planter shall have a maximum height of 2 feet and have a rounded bowl shape with a base as shown below. See chart below for the minimum number of planters required.

Building Front horizontal measurement	Planters required
Up to 50 ft	2
51' – 100'	4
101 – 125'	5

For each additional 25 feet one planter is required.

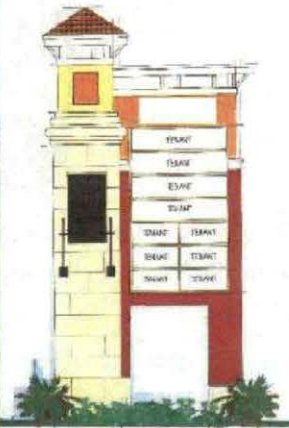


Reflective glass may not be used on the first floor of any building to prevent glare to vehicular traffic.

Roofs shall be barrel tile design or metal roof as designated per AC #1 Architectural Guidelines.



Signs shall be in keeping with the Central Parc Architectural Guidelines with columns, a base and capital with wording between columns. Calculations for sign faces are found in the City's Unified Land Development Code. The height of freestanding signs in Central Parc (Activity Center #9) is limited to a maximum of 25 feet.



Hangings signs are permitted with the bottom of the sign measuring a minimum of 8 feet above the sidewalk. The sign shall be oval in shape with a background color of Pantone warm gray 1C with terracotta color (Pantone 1605C) lettering and border. The lettering shall be Block. The sign shall be attached with a single arm with scroll embellishment as shown with a sign area not to exceed nine square feet.



Prohibited Signs are as follows:

- Animated signs
- Signs with visible neon tubing
- Bench signs
- Obscene signs
- Roof signs
- Painted wall signs
- Portable changeable copy signs
- Balloons of all types
- Revolving or rotating signs
- Flashing or audible signs

Streets trees are required along all roadways as regulated in the ULDC, Chapter 45. All shrubs shall be 40 inches above street grade when abutting a roadway. Where a commercial development abuts a residential neighborhood and not adjacent to a waterway or park lands, a solid masonry buffer wall shall be used in conjunction with trees placed 30 feet on center. Shrubs shall be planted between the trees to build a continuous hedge. A buffer shall be placed along each property line that abuts differing land uses with shrubs 36 inches in height at time of planting. All shrubs shall be from the approved shrub list in Chapter 21 of the ULDC.

Identifying Tree: Black Olive

Trash Receptacles shall be placed at all door entries and next to each pedestrian bench. The design shall be rounded and in a coordinating pattern, color and material as the planters to enhance visual impact throughout the Activity Center. See examples below.



Utilities shall be underground.

Wheel stops shall be placed in compliance with Chapter 21 and 25 of the Unified Land Development Code.