

Village D Village District Pattern Plan (West Villages A/K/A Wellen Park)

Final VDPP

Prepared For:

Main Street Ranchlands, LLLP

September 26, 2022

History of Approvals:

- a) **West Villages Town Center (Village D) Village District Pattern Plan, as approved September 26, 2017 (Petition No. VDC-15-159)**
- b) **West Villages Town Center (Village D) Village District Pattern Plan, as amended May 11, 2021, Ordinance No. 2021-18 (Petition No. VPA-20-239)**
- c) **Village D Village District Pattern Plan (West Villages AKA Wellen Park), as amended April 25, 2023, Ordinance No. 2023-04 (Petition No. VPA-22-180)**

Prepared By:



Amended By:



Village D Village District Pattern Plan (VDPP)

Owner:

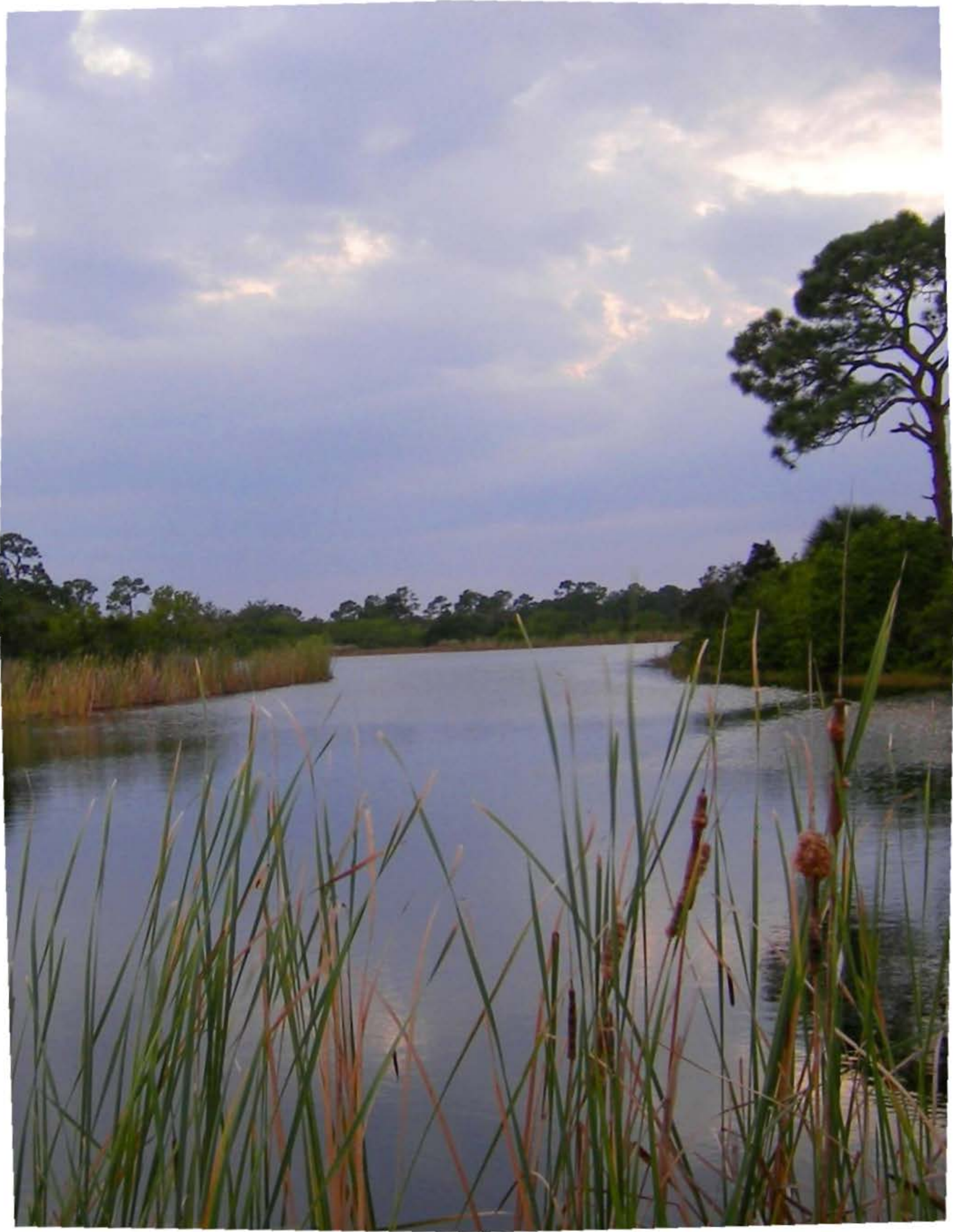
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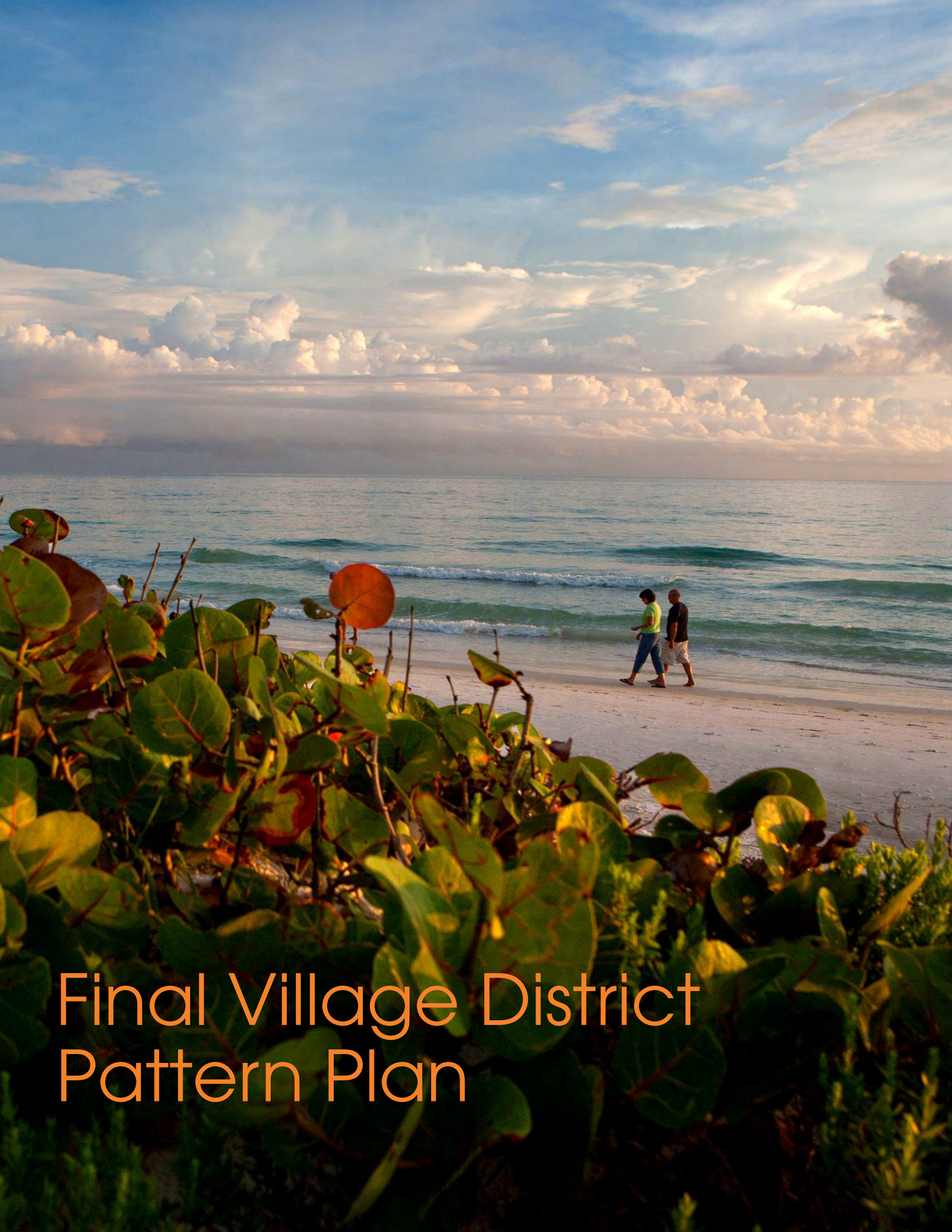
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Please note: This Plan contains references to West Villages also known as Wellen Park. Any reference to West Villages contained herein are to clarify the nomenclature of the Wellen Park community.



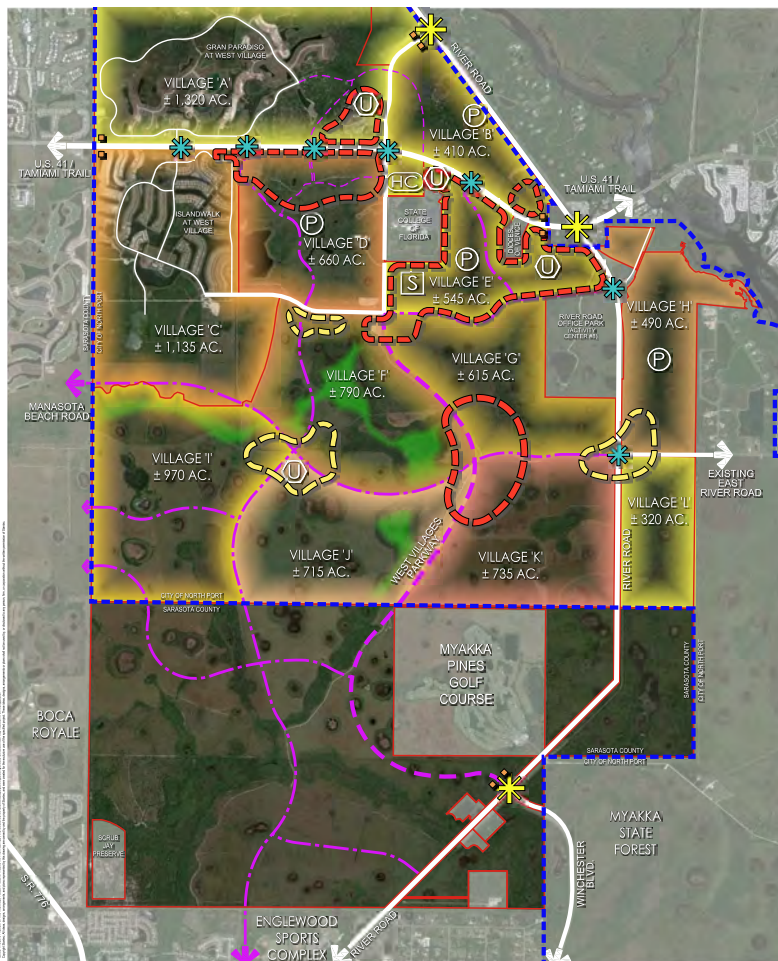
Final Village District Pattern Plan

Community Goals

3.1 Community Goals

Village D furtheres the following community goals:

- Create distinct neighborhoods with a vibrant, mixed-use town center as a focal point for the community.
- Offer diversity in housing types to attract a variety of residents of varying socio-economic status.
- Create a Town Center that combines commercial, residential, or recreational uses.
- Provide a mix of uses within a safe, walkable distance, to encourage use of non-vehicular transportation.
- Build a community that preserves and conserves environmentally protected areas.
- Build a community with a variety of open space options, which may include squares, greens, parks, and multi-use trails.



	EXISTING ROADWAY		APPROXIMATE SCHOOL LOCATION (K-12)		RECREATIONAL / OPEN SPACE / RECREATIONAL TRAIL SYSTEM		APPROXIMATE TOWN CENTER LOCATION (2)
	4 LANE W/ NEV & 8' SIDEWALKS		SARASOTA MEMORIAL HOSPITAL - HEALTH CARE SERVICES		POTENTIAL ROADWAY CONNECTION TO ADJACENT COMMUNITY		APPROXIMATE VILLAGE CENTER LOCATION (3)
	2 LANE W/ NEV, 8' SIDEWALK & 5' SIDEWALK		APPROXIMATE UTILITIES / PUBLIC FACILITIES LOCATION (10)		STATE COLLEGE OF FLORIDA		EXISTING TRAFFIC SIGNAL
	2 LANE W/ NEV & 5' SIDEWALKS		APPROXIMATE PARK LOCATION (10)		CITY OF NORTH PORT - CITY LIMITS		APPROXIMATE FUTURE FULL MEDIAN OPENINGS TRAFFIC CONTROL T.S.D.
	POTENTIAL WEST VILLAGES GATEWAY LOCATIONS						

- NOTES**
- (1) ACREAGES ARE APPROXIMATE. FINAL ACREAGE LOCATIONS SUBJECT TO REFINEMENT BASED UPON EXISTING SITE CONDITIONS AND FINAL VILLAGE DISTRICT PATTERN PLANS.
 - (2) AREAS DEFINED WITHIN THIS DESIGNATION WILL INCLUDE 300-1,000 ACRES OF TOWN CENTER AND MAY CONTAIN A VILLAGE AT THE DENSITY / INTENSITY LEVELS FOR TOWN CENTER AND DESIGNED ACCORDING TO THE VILLAGE DESIGN PRINCIPLES (53-212 (ULDC)). FINAL LOCATIONS WILL BE DETERMINED DURING THE V.D.P.P. PROCESS.
 - (3) AREAS DEFINED WITHIN THIS DESIGNATION WILL INCLUDE VILLAGE CENTER AT THE DENSITY / INTENSITY LEVELS FOR VILLAGE CENTER AND DESIGNED ACCORDING TO THE VILLAGE DESIGN PRINCIPLES (53-209(ULDC)). FINAL LOCATIONS WILL BE DETERMINED DURING THE V.D.P.P. PROCESS.
 - (4) LOCATION AND CONFIGURATION OF PARKS SHALL BE DETERMINED BY AGREEMENT BETWEEN THE PROPERTY OWNER AND THE CITY OF NORTH PORT.
 - (5) 12' MULTI-MODAL TRAILS MAY BE SATISFIED BY AN 8' SIDEWALK AND 4' BIKE LANES, AND SHALL BE FURTHER REFINED WITH FUTURE VILLAGE DISTRICT PATTERN PLANS.
 - (6) 8' - 12' TRAILS WILL BE DELINEATED ON V.D.P.P., AND MAY BE COMBINED WITH INTERNAL ROADWAYS.
 - (7) THE WEST VILLAGES IMPROVEMENT DISTRICT WILL COORDINATE WITH THE APPLICABLE GOVERNING AUTHORITY TO ESTABLISH ACCESS MANAGEMENT CRITERIA.
 - (8) FINAL LOCATIONS OF VILLAGE CENTERS, PUBLIC FACILITY SITES, SCHOOL SITES, VILLAGE BOUNDARIES AND ROADWAY CORRIDORS SHALL BE DETERMINED DURING THE V.D.P.P. PROCESS.
 - (9) FINAL ALIGNMENT OF CORRIDORS / ROADWAYS IS SUBJECT TO FINAL APPROVAL OF JURISDICTIONAL AGENCIES.
 - (10) GOVERNMENT USES, ACTIVITIES AND FACILITIES AND OTHER PUBLIC USES, ACTIVITIES AND FACILITIES, INCLUDING GOVERNMENT ADMINISTRATIVE BUILDINGS AND MAINTENANCE FACILITIES, POLICE STATIONS, FIRE STATIONS, SCHOOLS, LIBRARIES, PARKS, RECREATIONAL OR SPORTS FACILITIES, AREAS AND BUILDINGS; UTILITIES; INFRASTRUCTURE AND OTHER LAWFUL GOVERNMENT USES OF A LIKE NATURE, SHALL BE SUBJECT TO REVIEW BY THE CITY OF NORTH PORT THROUGH THE MAJOR SITE AND DEVELOPMENT PLAN PROCEDURES CONTAINED IN CHAPTER 33, CITY OF NORTH PORT UNIFIED LAND DEVELOPMENT CODE. SUCH GOVERNMENT USE, ACTIVITIES, AND FACILITIES BEING APPROVED THROUGH THE MAJOR SITE AND DEVELOPMENT PLAN PROCEDURES SHALL BE EXEMPT FROM THE APPLICATION PROCESS CONTAINED IN SECTION 53-5.8, CITY OF NORTH PORT UNIFIED LAND DEVELOPMENT CODE.
- FINAL SIZE, LOCATION, AND CONFIGURATION OF UTILITIES/PUBLIC FACILITIES SHALL BE DETERMINED BY AGREEMENT BETWEEN THE PROPERTY OWNER AND THE CITY OF NORTH PORT.

Approved September 13, 2016
 ORDINANCE NUMBER 2016-17
 PETITION NUMBER VIA-15-178

West Villages Index Map

Final Village District Plan

3.2 Introduction

The Proposed Village District Plan for Village D was designed according to the Village District Performance standards contained in the City of North Port's Comprehensive Plan. It builds on the vision that was established during the Preliminary VDPP planning process, described in Chapter 2. Figure 3.2.A shows the Final Village Plan, including four mixed use areas that generally represent the Town Center, one Mixed Use Residential Neighborhood, and one residential neighborhood, as well as open space, environmental systems, and roadways. The design of the site utilizes the Site Analysis, as well as the broader concepts expressed in the West Villages Village District Pattern Book (VDPB) and Village Index Map.

The objectives of the VDPB are to establish a broad community framework, to encourage development interests the flexibility to express themselves through the development of sustainable Villages, without restrictive regulations that hamper creativity or adaptability to changing market conditions. To further these objectives, the West Villages Review Committee (WVRC) will be created. This committee will include one individual from Main Street Ranchlands, LLLP, the Master Developer, and two individuals appointed from the West Villages Improvement District. They will be responsible to review architectural features, design components, and landscape plans of retail/commercial, mixed use, and residential buildings and sites within Village D. The WVRC will use Section 10 of the West Villages VDPB, as well as design standards established for the Village D Town Center, contained in this Village District Pattern Plan (VDPP), for guidance in making approval decision. Unless otherwise indicated, the specifications relating to landscaping contained in the City of North Port Unified Land Development Code shall be the minimum standard the Design Review Committee is empowered to approve.

Village D is envisioned as a vibrant mixed use Town Center that includes a broad mix of retail, commercial, hotel, recreational, office and residential uses. The Proposed Village District Plan for Village D includes four mixed use areas located along the US 41 corridor and West Villages Parkway, which will contain a broad mix of uses, and is envisioned as the primary focus of the more intensive commercial uses. The plan includes one mixed use residential neighborhood, which may include a mix of office, retail, and residential uses. The plan also includes one residential neighborhood, which allows for a range of residential unit types. There is a purposeful transition in intensity from north to south and east to west. The transition is intended to ensure compatibility with the existing village of Islandwalk, to the west, as well as future development to the south and southeast.

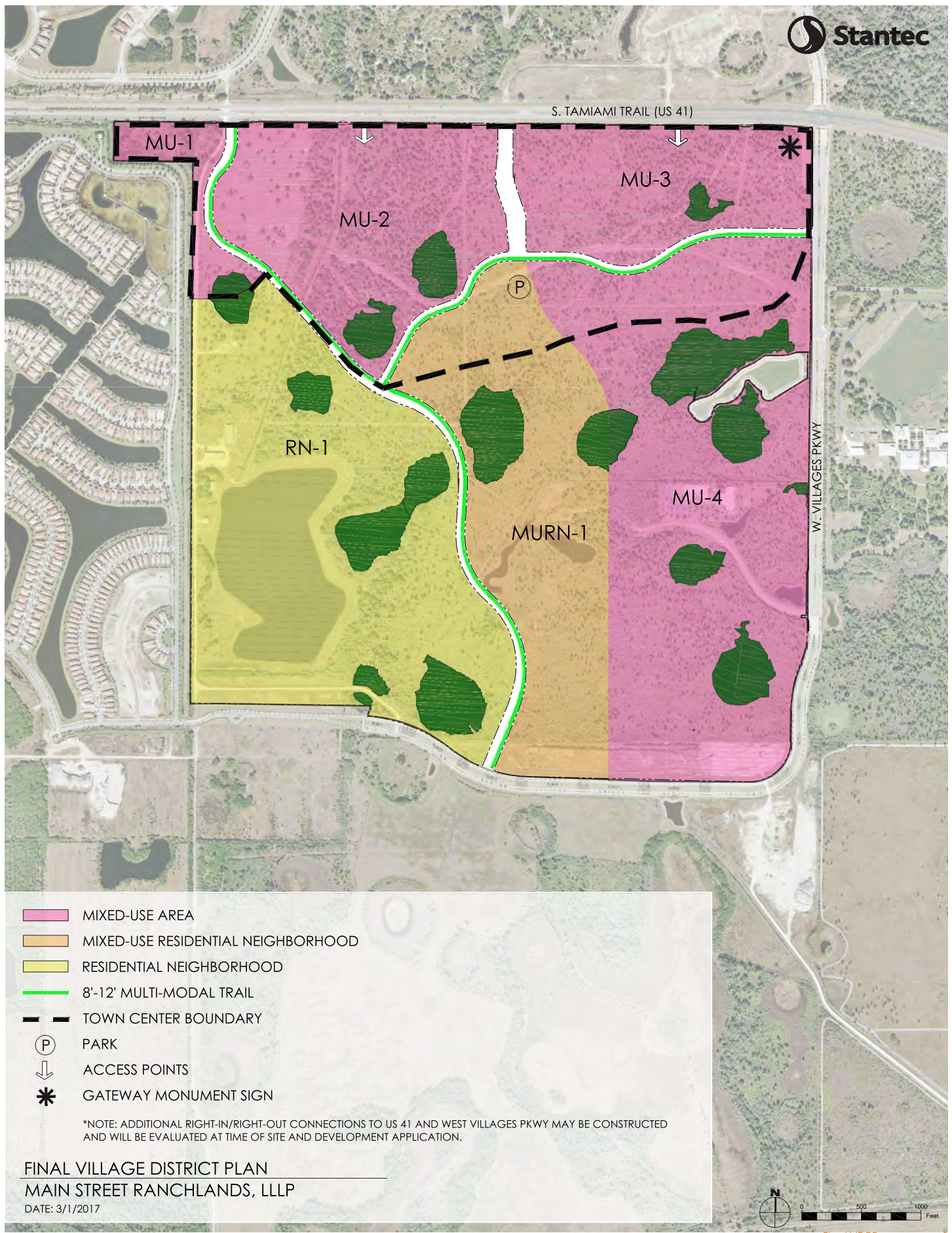
The Village Index Map includes a park within Village D. The location of the park is generally reflected on the Proposed Village District Plan. Vehicular access to the park will be provided via the internal roadway network and the multi-modal trail system.

The Village Index Map provides the conceptual framework for pedestrian and vehicular connections in and through the West Villages. The roadway alignments shown are consistent with the Sarasota County and City of North Port thoroughfares plans. The roadways, along with the continuation of the multimodal trails system, will provide connections in and between villages, as well as areas beyond the limits of the City of North Port.

Consistent with ULDC Section 53.219, where there are conflicts between this VDPP, ULDC provisions, the general land use, subdivision or other applicable regulations, the standards adopted and shown in the approved Pattern Book and this VDPP shall control.

Figure 3.2.B offers an illustrative example of development components anticipated for Village D. Although MU-4 is shown as residential development in the concept plan, the form of development could be consistent with what is shown along US 41. Likewise, the proposed high density mixed use development could be adjusted to accommodate a less intensive form of development.

Figure 3.2.A



S. TAMAMIAMI TRAIL (US 41)

MU-1

MU-2

MU-3

RN-1

MURN-1

MU-4

W. VILLAGES PKWY

- MIXED-USE AREA
- MIXED-USE RESIDENTIAL NEIGHBORHOOD
- RESIDENTIAL NEIGHBORHOOD
- 8'-12' MULTI-MODAL TRAIL
- TOWN CENTER BOUNDARY
- P PARK
- ↓ ACCESS POINTS
- ✱ GATEWAY MONUMENT SIGN

*NOTE: ADDITIONAL RIGHT-IN/RIGHT-OUT CONNECTIONS TO US 41 AND WEST VILLAGES PKWY MAY BE CONSTRUCTED AND WILL BE EVALUATED AT TIME OF SITE AND DEVELOPMENT APPLICATION.

FINAL VILLAGE DISTRICT PLAN
 MAIN STREET RANCHLANDS, LLLP
 DATE: 3/1/2017



Figure 3.2.B



LEGEND

	RETAIL / RESTAURANT		MIXED USE
	MULTI-FAMILY		SINGLE FAMILY
	HOTEL		
	OFFICE		

Note:
Designs are for illustrative purposes only and are not intended to be regulatory or limiting to layout, roadway networks, parking lots, open space, buffers, development plans, uses, or other design features.

Proposed Neighborhoods Plan

3.3 Neighborhood Character

Village D is intended to primarily serve as the Town Center for The West Villages, to include retail, office, civic and residential uses.

Three areas adjacent to US 41 are primarily envisioned for mixed use development, with a focus on residential, retail and commercial uses. These areas are identified as MU-1, MU-2 and MU-3 on Figure 3.2.A – Proposed Village Neighborhood Plan. The mixed use areas will primarily serve the commercial/retail and office needs of The West Villages. The proposed development standards are envisioned to permit a broad range of uses that will promote the long-term economic viability of the West Villages. Not only will this Town Center offer a variety of commercial and office development to meet the daily needs of residents, but it will also be made available for corporate centers and research and development offices or complexes, which will further the goals of the Village Land Use Category, by encouraging a better jobs/housing balance. Finally, this community is proposed to offer a variety of residential unit types. Residential units will be intermixed, either vertically, or separately. Townhouses, stacked townhouses, and multi-family units may also be developed within the Mixed Use areas.

The Mixed Use Residential Neighborhood is envisioned as a neighborhood that contains a broad range of non-residential and residential land uses. Commercial/retail and office uses are also envisioned within this neighborhood, as well as multi-family, townhouses, and single-family semi-detached and detached units. As development progresses in the Mixed Use Residential Neighborhood, it is anticipated that the existing structures will either be demolished, in favor of a different land use type, or adapted, in order to be woven into the fabric of the neighborhood.

One distinct moderate density residential neighborhood, identified as RN-1, is envisioned for Village D. The residential unit types, recreational amenities and conservation of protected environmental features will enhance the character of this neighborhood.

An important element of the village design includes a multi-modal trail network that connects areas to neighborhood centers, open space tracts, and the Town Center, identified as MU-1, MU-2, MU-3, and MU-4, as well as the park, generally identified on the Proposed Village Neighborhood Plan. These features facilitate citizen interaction by linking village areas and neighborhoods to on-site and adjacent amenities and facilities. These trails will provide residents with recreational

opportunities and access to the natural environment and open spaces.

This Village is designed to be a vibrant, mixed use area, offering a broad mix of residential unit types, which may be mixed vertically and horizontally with retail, commercial and office uses. The vision is to create an urban form in the Mixed Use Areas, with a slight reduction in intensity in the Mixed Use Residential Neighborhood, to traditional moderate density residential neighborhoods, making available a range of housing types, in Residential Neighborhood 1.

In furtherance of the interpretation authority granted by the City of North Port Comprehensive Plan and Land Development Code, the Zoning Administrator/Planning Manager shall have the authority to administratively approve modifications initiated by the Master Developer, of standards and the conceptual design layout contained within this VDPP Application. The Zoning Administrator/Planning Manager may impose reasonable mitigation measures to limit impacts from the requested adjustment of standards. The Zoning Administrator/Planning Manager shall have the added authority to administratively approve modifications to standards initiated by the Master Developer that provide a benefit to the general public or surrounding community, as long as those modifications do not increase density, maximum allowable building heights, reduce buffer widths along US 41, or add permitted uses. All standards contained herein may be met throughout the overall VDPP area and not individual parcels or lots. As an example, all tree canopy will be applied to the total area of Village D.

3.4 Structure Types

Village D is planned to include a range of housing types, which may include Single-Family Detached, Single-Family Semi-Detached, or Paired Villas, Single-Family Attached Townhouses with front or side-loaded garages, Single-Family Attached Townhouses with rear loaded garages, Stacked Townhouses, Multi-Family, and residential units within Mixed-Use buildings. See Development Standards in Figure 3.4.A and 3.4.B for dimensional standards.

A Single-Family detached unit is a standalone house, not attached to any other dwelling by any means, and surrounded by open space or yards. Lot sizes vary, to allow a variety of usable private yard space and building separation. An accessory apartment is a permitted use on Single-Family Detached lots, and allows an accessory dwelling unit with a separate means of ingress and egress containing a separate kitchen, bathroom and sleeping facilities that is either physically attached to, or contained within an existing single-family house or occupies the second story of an existing garage or accessory building on

the same lot as the principal dwelling. Accessory apartments will not count as a dwelling unit for density calculation purposes.

A Single-Family semi-detached unit is a one-family dwelling attached to one other one-family dwelling by a common vertical wall, with each dwelling located on a separate lot. Parcels contain one side yard on the opposite side of the common wall and a front and rear yard.

A Single-Family Attached Townhouse is a one-family dwelling in a row of at least three such units in which each unit has its own front and rear access to the outside, no unit is located over another unit, and each unit is separated from any other unit by one or more vertical common fire-resistant walls. Neighborhoods may contain Single-Family Attached units with front or side-loaded garages, or rear loaded garages, where an alley exists.

A stacked Townhouse typically has two units stacked vertically.

A Multi-Family unit contains three or more dwelling units, which is typically two or more stories tall.

A Mixed-Use dwelling is one that is located within a mixed-use building, which contains a variety of complementary uses in a single building.

Maximum density for West Villages has been established. Comprehensive Plan Policy 13.1.n establishes that 16,000 dwelling units is the maximum number of units that may be developed in areas of the West Villages that lie within the City of North Port. To demonstrate compliance with this requirement, future Site and Development Plans and/or Plat applications will include a tracking chart, prepared by the Master Developer, to indicate the allocation of units to individual developments within the West Villages. Residential density will be calculated for the land areas identified on Figure 3.2.B (Mixed Use Residential Neighborhood, Residential Neighborhood, and Mixed Use Area). The maximum density shown on Figure 3.4.A, Development Standards, may be exceeded for an individual project, as long as the overall density for the area (MURN, RN, MU) remains at or below the established density for that area. Public, non-profit, and institutional uses are permitted in all areas and shall not count toward non-residential or residential intensity or density.

If, during development, it is found that transportation impacts Village D are more than what was contemplated in the Traffic Impact Analysis for Village D, additional analysis will be conducted.

**Figure 3.4.A
Main Street Ranchlands Development Standards**

	MIXED USE RESIDENTIAL NEIGHBORHOOD	RESIDENTIAL NEIGHBORHOOD	MIXED USE AREA
(+/-) ACRES	109	174	339
(+/-) ADJUSTED GROSS ACRES	100	135	307
OPEN SPACE (+/-) ACRES	9	39	33
FLOOR AREA RATIO (3) DENSITY LIMITATIONS (4)	2.0 FAR 16 Dwelling units per Acre	0.25 FAR 4 Dwelling units per Acre	3.0 FAR 24 Dwelling units per Acre
Permitted Uses(1)(6)(7)	Community Center, Gatehouse, Single-Family Detached Type A&B, Single-Family Semi-Detached, Townhouses, Stacked Townhouses, Multi-Family, Model Homes/Sales Center, Residential Support Uses, Retail, Commercial/Service, Parking/Utility/Communication/ Essential Services	Community Center, Gatehouse, Single Family Detached - Type A and Type B, Single Family Attached, Townhouses, Stacked Townhouses, Multi-Family, Model Homes/Sales Center, Residential Support Uses, Parking/Utility/Communication/ Essential Services	Community Center, Gatehouse, Single-Family Detached Type A&B, Townhouses, Stacked Townhouses, Multi-Family, Residential Support Uses, Retail, Commercial/Service, Parking/Utility/Communication/ Essential Services
Minimum Lot Size	See Figure 3.4.B.; Non-residential - no min. lot area	See Figure 3.4.B; Non-residential - no min. lot area	See Figure 3.4.B.; Non-residential - no min. lot area
Maximum Structure Height	42 Feet (s.f.), 80 Feet (townhouses, community center, gatehouse), 120 feet (hotel/motel, multifamily, nonresidential)	42 Feet (s.f.), 60 Feet (townhouses, multi-family, community center, gatehouse, nonresidential)	50 Feet (s.f.) 60 feet (community center, gatehouse, townhouse), 120 Feet (multi-family, hotel/motel, non-residential)
Setbacks(2)(5)	Residential - See specific structure type in Section 3.4.B Non-residential(4) - Meet State Building & Fire Code	Residential - See specific structure type in Section 3.4.B. Non-residential(4) - Meet State Building and Fire Code	Residential - See specific structure type in Section 3.4.B Non-residential - Meet State Building and Fire Code

Notes:

- (1) Above ground utility structures shall be allowed anywhere within the Village provided that such facilities incorporate adequate levels of buffers to appropriately protect enjoyment on adjacent uses.
- (2) Fences, walls, columns, entry monumentation, decorative features, and utility facilities such as lift stations, storage tanks, ground mounted transformers and wells shall be exempt from any setback standards. A berm up to 8' in height may be constructed as part of a buffer. Up to 8' in height of wall or fence may be constructed with or without a berm as part of the landscape or buffering plans.
- (3) Floor to area ratio (FAR) standards shall be calculated for the land areas identified on the Village District Plan (MURN, MU) (Fig. 3.2.A). With each Site & Development and/or Plat Application, a Tracking Chart will be provided to demonstrate compliance with the required Land Use Mix (Fig. 3.13.1).
- (4) Residential density shall be calculated for the land areas identified on the Village District Plan (MURN, RN, MU) (Fig. 3.2.A). With each Site & Development and/or Plat Application, a Tracking Chart will be provided to demonstrate compliance with the overall maximum density of West Villages, and to ensure compliance with the required Land Use Mix (Fig. 3.13.1).
- (5) Setbacks may be reduced to 0 feet when the subject parcel is adjacent to public/private right-of-way, easement, open space tract or water body that is at least 30 ft in width. Air conditioning units, pool pumps and other mechanical equipment shall be permitted in side yard setbacks.
- (6) Utility structures may be located in easements or in right-of-ways as indicated in roadway cross-sections.
- (7) Lakes and ponds may be used for irrigation and or storage of reclaimed water.

Figure 3.4.B. Typical Configurations for Structures

The specific notes referenced below apply to the development standards outlined on the following pages for the structure types listed below.

Single-Family - Detached Type A
 Single-Family - Detached Type B
 Single-Family - Semi-detached Paired Villa
 Single-Family - Attached Townhome (Front Loaded)
 Single-Family - Attached Townhome (Rear Loaded)
 Multi-Family
 Mixed-Use

Development Standards	Notes
Lot Area Min.	9
Lot Width Min.	8,9
Lot Depth Min.	
Lot Coverage Max.	10
Front Setback Min. (Measured to sidewalk)	1
Side Setback Min. (Principal Structure)	3,4,7,8,11
Side Setback Min. (Pool Deck, Screen Enclosure)	2,3,7,8,11
Rear Setback Min. (Principle Structure)	5,7
Rear Setback Min. (Pool Deck, Screen Enclosure)	5,7

Notes

- 1 Front Loading Garage (FLG), Side Loading Garage (SLG), Recessed Garage Option (RGO)
- 2 Screen enclosures for Townhomes and Single-Family Attached (SFA) units will have a 5 FT side setback without a privacy wall, or a 0 FT side setback with a min. 6 FT high privacy wall, provided that the screen is located atop the wall.
- 3 Patios and pool decks for Townhomes and SFA units may have a 0 FT side setback provided units abut a shared privacy wall.
- 4 Corner setbacks do not apply when the side property line is adjacent to a platted open space tract of at least 5 FT in width.
- 5 The rear setback may be reduced to 0 FT when the rear property line abuts an easement, water body, or open space tract of at least 30 FT in dimension.
- 6 Townhome units may include two or more attached units.
- 7 Cornices, veneers, or other non-structural projections shall not count towards setbacks and shall be treated similar to roof overhangs.
- 8 Side yard setbacks for Single-Family Detached units are a combined 10 FT.
- 9 Minimum lot area and width for curvilinear lots may be less than required provided that all min. setback requirements are met and the average lot width (front lot line and rear lot line) is equal to or greater than the min. lot width required. Irregular lot dimensions that meet minimum lot size are permitted.
- 10 Lot coverage is defined as the percent of lot area under fixed roof. Lot coverage does not include pools, decks, driveways, patios, sidewalk, etc.
- 11 Air-conditioning units and mechanical equipment shall be allowed in side yard setbacks no closer than 1.5' from lot line.
- 12 For Single-Family Detached type A and B, A is provided for illustrative purposes. B reflects the minimum dimensional standards.
- 13 FLG setback is to the garage face. SLG and RGO setback minimum shall apply to the non-garage portion of the structure.

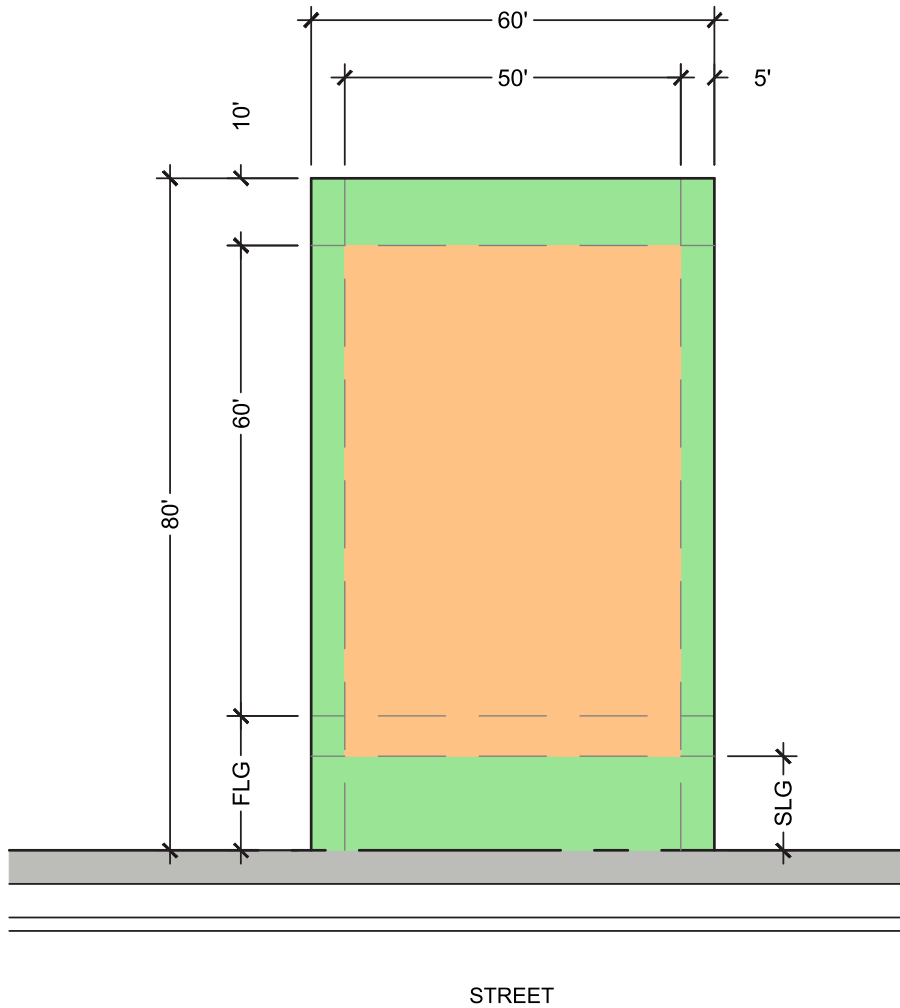
SINGLE-FAMILY

DETACHED - TYPE A

Development Standards

Lot Area Min.	4,800 SF
Lot Width Min.	60 FT
Lot Depth Min.	80 FT
Lot Coverage Max.	65%
Front Setback Min. (Measured to sidewalk)	20 FT (FLG) / 10 FT (SLG)(RGO)
Side Setback Min. (Principal Structure)	10 FT Between Structures, 10 FT Corner Lot
Side Setback Min. (Pool Deck, Screen Enclosure)	3 FT Pool; 0' Pool Deck/Screen
Rear Setback Min. (Principle Structure)	10/5 FT (RGO)
Rear Setback Min. (Pool Deck, Screen Enclosure)	0 FT (deck/screen) / 3 FT (Pool Edge)

Single-Family Detached (Type-A)



SINGLE-FAMILY

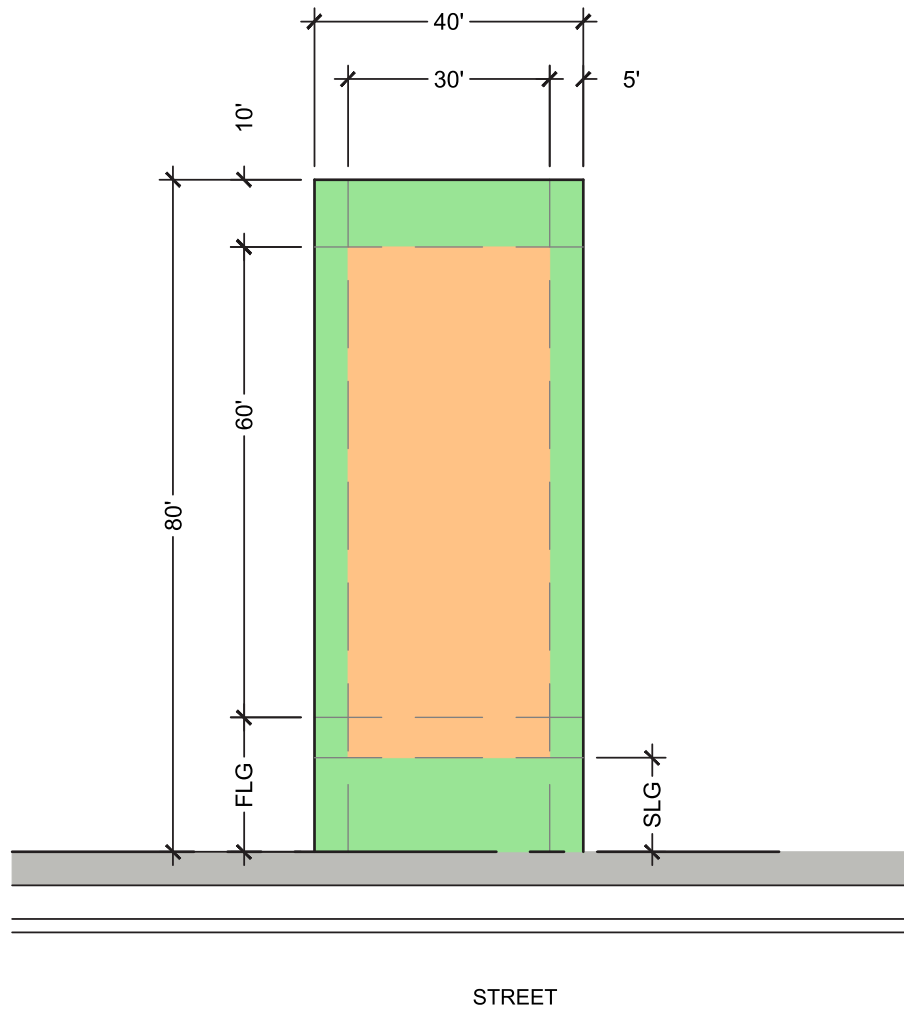
DETACHED - TYPE B

Development Standards

- Lot Area Min.
- Lot Width Min.
- Lot Depth Min.
- Lot Coverage Max.
- Front Setback Min. (Measured to sidewalk)
- Side Setback Min. (Principal Structure)
- Side Setback Min. (Pool Deck, Screen Enclosure)
- Rear Setback Min. (Principle Structure)
- Rear Setback Min. (Pool Deck, Screen Enclosure)

Single-Family Detached (Type-B)

- 3,200 SF
- 40 FT
- 80 FT
- 65%
- 20 FT (FLG) / 10 FT (SLG)(RGO)
- 10 FT Between Structures, 10 FT Corner Lot
- 3 FT Pool; 0 FT Pool Deck/Screen
- 10/5 FT (RGO)
- 0 FT (Pool Deck) / 3 FT (Pool Edge)



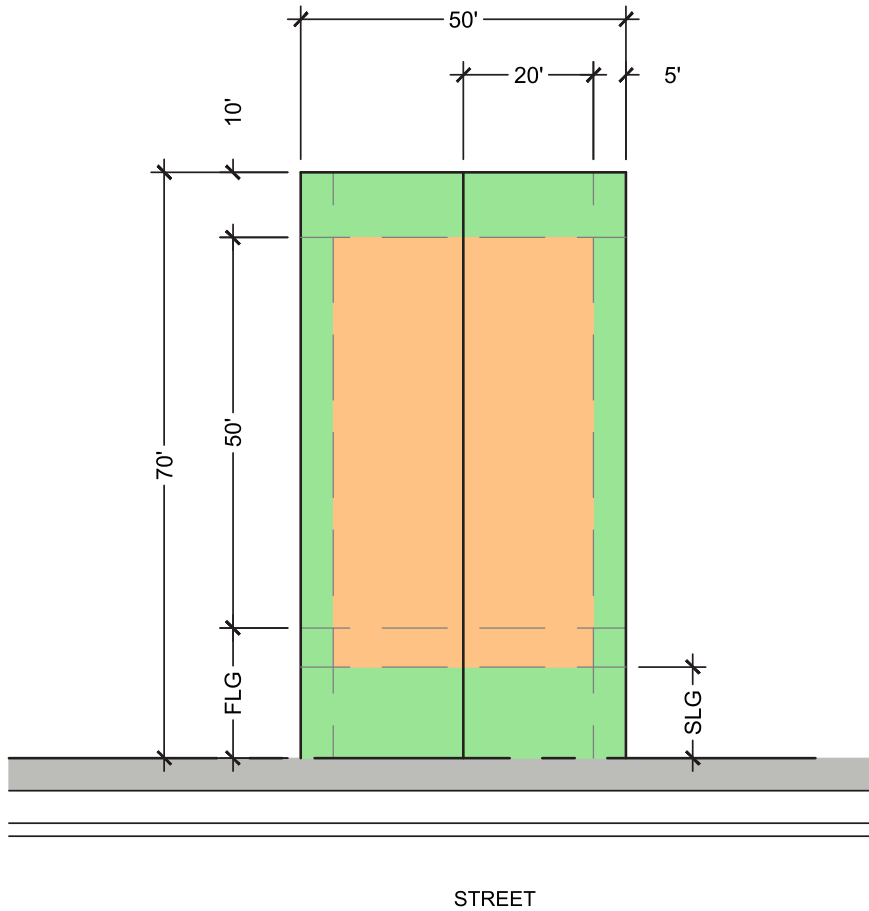
SINGLE-FAMILY
SEMI-DETACHED - PAIRED VILLA

Development Standards

- Lot Area Min.
- Lot Width Min.
- Lot Depth Min.
- Lot Coverage Max.
- Front Setback Min. (Measured to sidewalk)
- Side Setback Min. (Principal Structure)
- Side Setback Min. (Pool Deck, Screen Enclosure)
- Rear Setback Min. (Principle Structure)
- Rear Setback Min. (Pool Deck, Screen Enclosure)

Single-Family Semi-detached (Paired Villa)

- 1,750 SF (per unit)
- 25 FT
- 70 FT
- 75%
- 20 FT (FLG) / 10 FT (SLG)(RGO)
- 10 FT Between Structures (0 FT Common Wall/Shared Lot Line) / 10 FT Corner Lot
- 3 FT (0 FT Common Wall/Shared Lot Line) / 5 FT (Side Yard Pool Edge)
- 10 FT
- 0 FT (deck/screen) / 3 FT (Pool Edge)



SINGLE-FAMILY

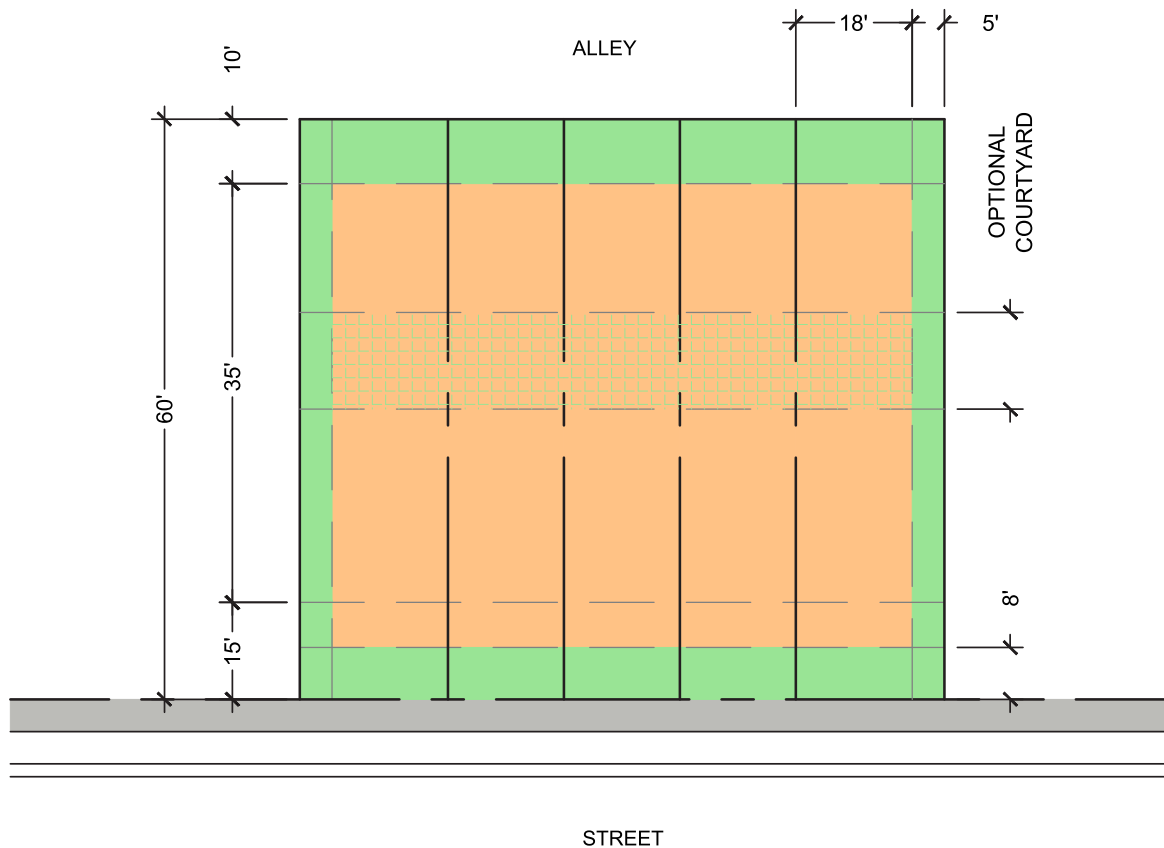
ATTACHED - TOWNHOME (ALLEY-LOADED)

Development Standards

Lot Area Min.
 Lot Width Min.
 Lot Depth Min.
 Lot Coverage Max.
 Front Setback Min. (Measured to sidewalk)
 Side Setback Min. (Principal Structure)
 Side Setback Min. (Pool Deck, Screen Enclosure)
 Rear Setback Min. (Principle Structure)
 Rear Setback Min. (Pool Deck, Screen Enclosure)

Single-Family Attached (Townhome) (6)

1,080 SF (per unit)
 18 FT
 60 FT
 80%
 7 FT / 0 FT (Porch/Stoop)
 5 FT (0 FT Common Wall/Shared Lot Line) / 10 FT Corner Lot
 4 FT (0 FT Common Wall/Shared Lot Line) / 5 FT (Side Yard Pool Edge)
 5 FT
 N/A



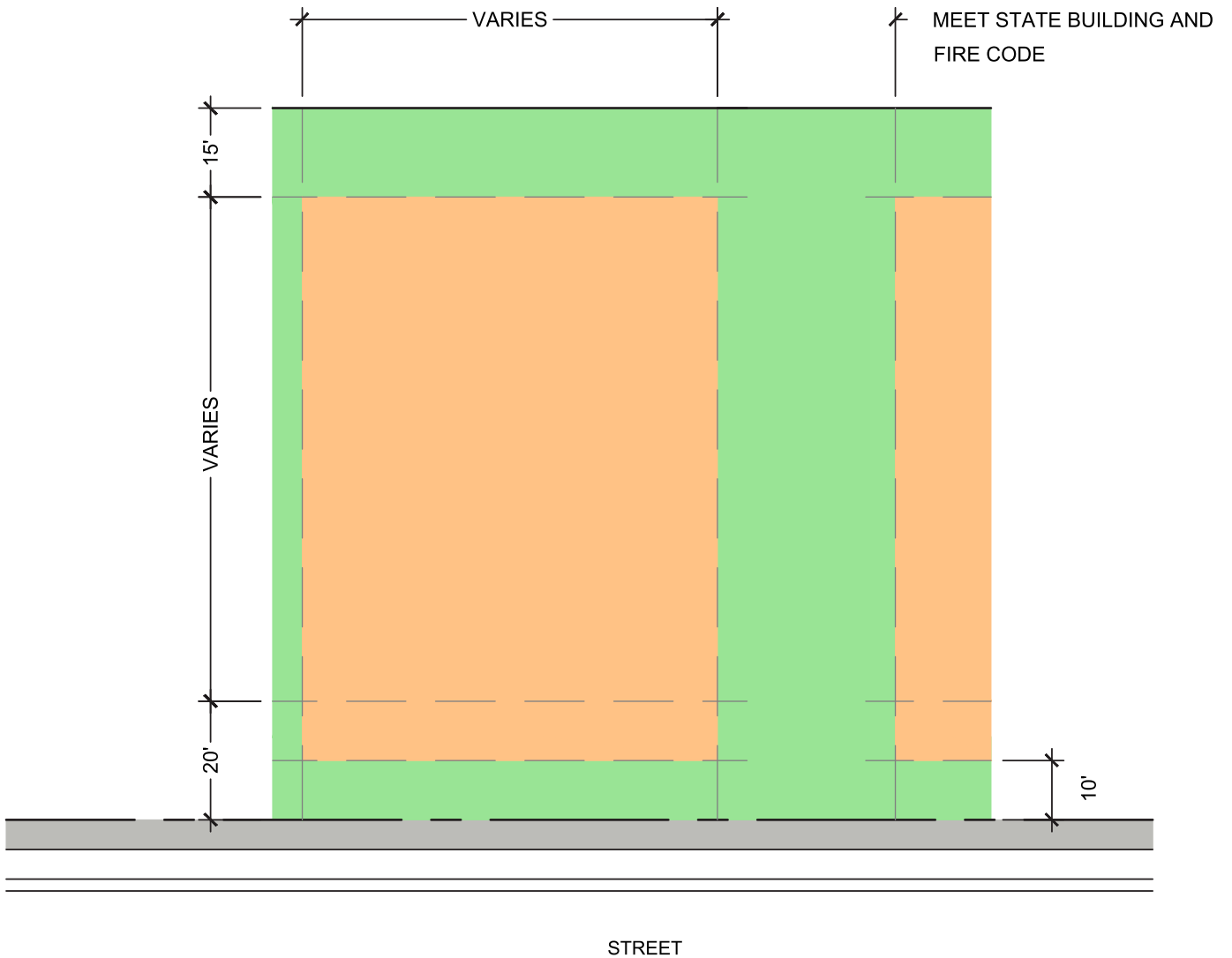
MULTI-FAMILY/STACKED TOWNHOUSE

Development Standards

- Lot Area Min.
- Lot Width Min.
- Lot Depth Min.
- Lot Coverage Max.
- Front Setback Min. (Measured to sidewalk)
- Side Setback Min. (Principal Structure)
- Side Setback Min. (Pool Deck, Screen Enclosure)
- Rear Setback Min. (Principle Structure)
- Rear Setback Min. (Pool Deck, Screen Enclosure)

Multi-Family

- N/A
- N/A
- N/A
- N/A
- 0' / 20 FT (FLG) / 10 FT (SLG)(RGO)
- Meet State Building and Fire Code
- N/A
- 15 FT
- N/A



MIXED-USE

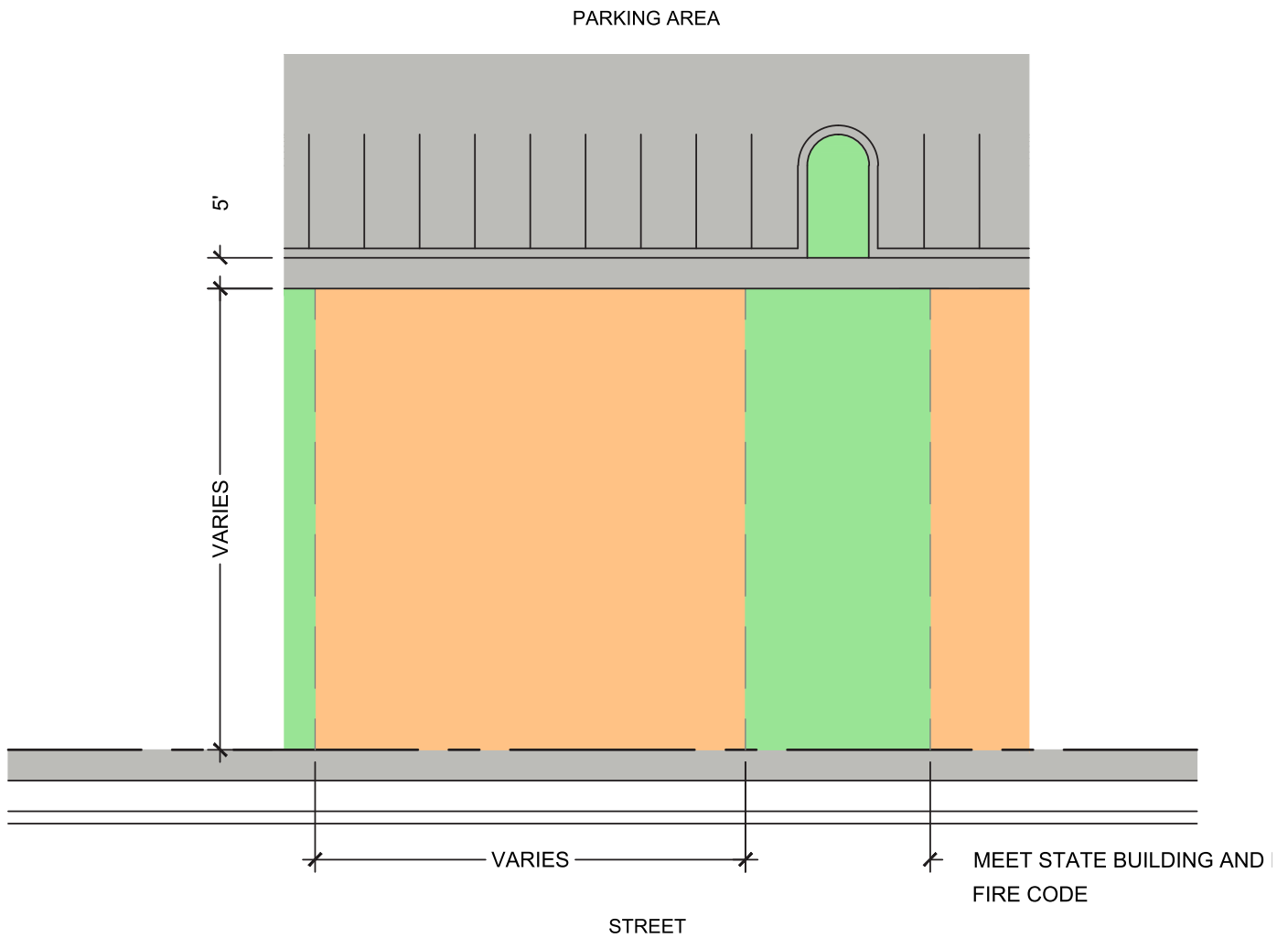
RESIDENTIAL / NON-RESIDENTIAL

Development Standards

- Lot Area Min.
- Lot Width Min.
- Lot Depth Min.
- Lot Coverage Max.
- Front Setback Min. (Measured to sidewalk)
- Side Setback Min. (Principal Structure)
- Side Setback Min. (Pool Deck, Screen Enclosure)
- Rear Setback Min. (Principle Structure)
- Rear Setback Min. (Pool Deck, Screen Enclosure)

Mixed-Use

- N/A
- N/A
- N/A
- N/A
- 0 FT
- Meet State Building and Fire Code
- N/A
- N/A
- N/A



3.5 Neighborhood Centers

Neighborhood centers are intended to serve residential neighborhoods and may provide recreation, neighborhood scale commercial, office, or civic uses. Village D anticipates that each of the neighborhood centers will maintain the character of the neighborhood, and will offer a range of uses to create variety and function within the village.

Neighborhood centers in Residential Neighborhood-1 may be designed as amenity centers, passive or active parks, neighborhood greens, or civic nodes. These neighborhoods will be designed, so homes are generally within a half mile radius of the neighborhood center.

Consistent with the proposed Index Map, Mixed Use Residential Neighborhood-1 includes a park that will have both active and passive recreational opportunities for residents of The West Villages. Vehicular access will be provided, and it will be accessible by residents via the multi-modal trail.

Proposed Town Center

3.6 Town Center

Village D contains four areas dedicated to Town Center development, which will offer residents of The West Villages access to a broad range of retail/commercial, office, civic, and residential uses. To insure commercial development does not exceed market demand, no Village Centers are proposed within Village D.

Roadways and Pathways

3.7 Roadways and Pathways

Roadways within Village D will comfortably accommodate vehicular, pedestrian, Neighborhood Electric Vehicles, and bicycle traffic. Through design and development of Village D, the proposed maximum posted speed will be up to 35 miles per hour, to encourage the use of Neighborhood Electric Vehicles as an alternative mode of transportation for trips within the West Villages. A sidewalk system will be constructed to facilitate pedestrian circulation. In addition, roadways will be landscaped and lit to enhance the community appearance and contribute to pedestrian comfort. Described below are six types of roadways and pathways that may be implemented for the village development: Main Streets, Mixed-Use Streets, Avenues, Residential Neighborhood Streets, Alleys, and Multi-Modal Trails. Typical cross sections are depicted in the subsequent sections. The Roadways and Pathways depict improvements that are impact fee and mobility fee creditable, and are to be constructed and maintained by the WVID and/or the Master Developer. The phasing of roadways within Village D will be consistent with proposed phasing within Village D to provide sufficient and safe access concurrent with the development, as determined by the Master Developer.

Roadways and Pathways

Main Streets (Figure 2.6.1.A-B) are generally slow movement local roadways that offer a pedestrian environment and on-street parking for the commercial Main Street. Certain areas also provide for free movement, at intersections to arterials. These roads are intended to provide connectivity to other Villages and arterials.

Figure 3.7.1.A.

Main Street At US41 / Boulevard (Preto)

Main Street as it intersects US Highway 41 is a free movement thoroughfare as it enters a full access intersection. Preto Boulevard is a 4 lane divided thoroughfare.

- Free movement thoroughfare as it enters a full access intersection.
- Typically a two- to four-lane divided roadway with turn lanes.
- Provides regional connections to the other villages and arterials.
- May be designed for up to 35 mile per hour speed limit.
- May include trees and landscaping in median and public frontages.
- Designed to accommodate pedestrian, bicycle, and Neighborhood Electric Vehicles.

Design Parameters	Min.	Max.
Target Speed	15	35
Travel Way Dimensions		
Travel Lane(s) Width (ft)	11	14
Turn Lane Width	10	12
Bike Lane Width (ft)	5	7 (Buffered)
Parking Lane Width (Parallel Parking) (ft)	N/A	N/A
Parking Lane Width (Angled Parking) (ft)	N/A	N/A
Public Frontage		
Planting Type	Planted/Grass	
Planting Width (ft)	0	-
Walkway Width (each side) (ft)	5	-
Right Of Way		
ROW Width	100	-

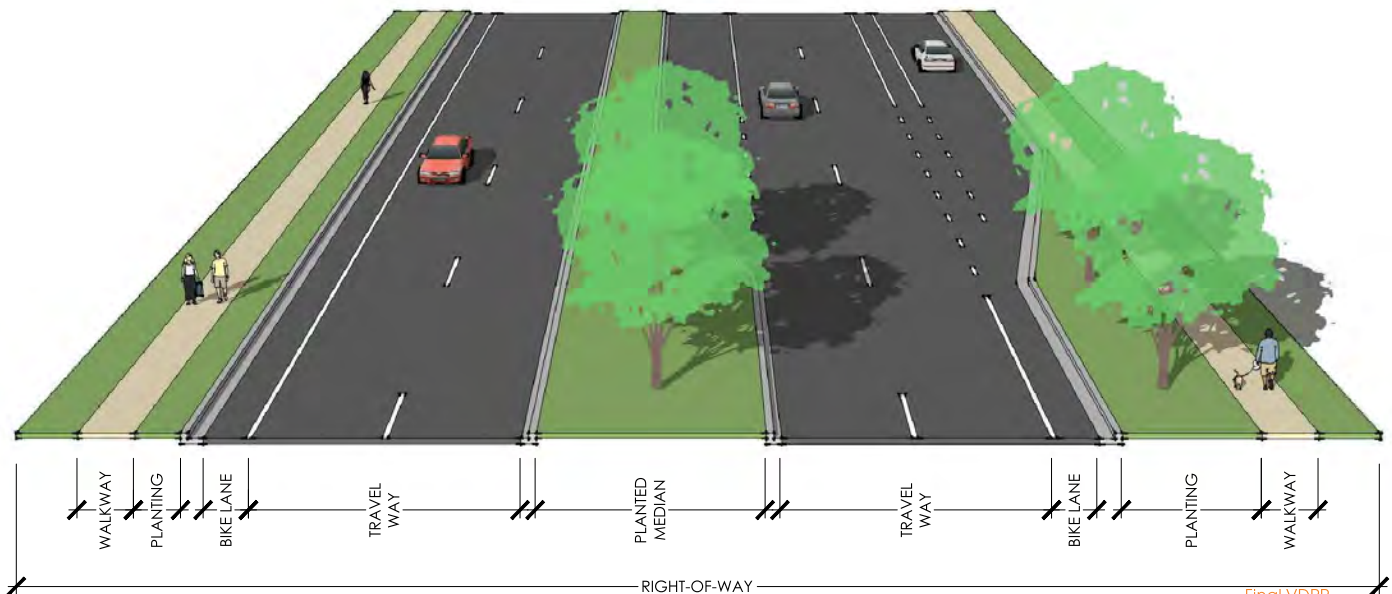


Figure 3.7.1.B.

Main Street

Main Street is a slow movement local roadway providing a pedestrian environment and on-street parking for the commercial main street.

- Slow movement local roadway that offers a pedestrian environment for the commercial Main Street.
- Typically a two-lane divided roadway with angled or parallel parking on either side of the travel lane.
- May be designed for 25 mile per hour speed limit.
- May provide for public frontages with plantings containing trees.
- May provide mid-block un-signalized walkways to promote pedestrian connectivity.
- Designed to accommodate pedestrian, bicycle and Neighborhood Electric Vehicles.

Design Parameters	Min.	Max.
Target Speed	15	25
Travel Way Dimensions	Min.	Max.
Travel Lane(s) Width (ft)	12	17
Parking Lane Width (Parallel Parking) (ft)	8	9
Parking Lane Width (Angled Parking) (ft)	18	21
Public Frontage	Min.	Max.
Planting Type	Intermittent	
Planting Width (ft)	0	6
Walkway Width (ft)	10	-
Right Of Way	Min.	Max.
ROW Width	120	200

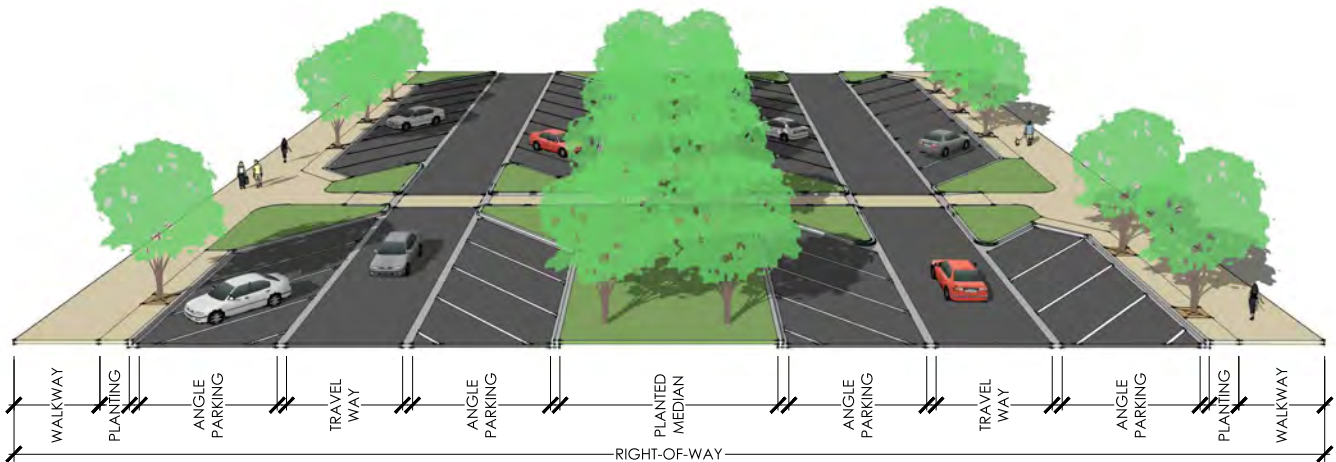


Figure 3.7.1.C.

Main Street Alternative

Main Street Alternative is a slow movement local roadway providing a pedestrian environment.

- Slow movement local roadway.
- Typically a two-lane undivided roadway.
- May be designed for 25 mile per hour speed limit.
- May provide for public frontages with plantings containing trees.
- May provide mid-block un-signalized walkways to promote pedestrian connectivity.
- Designed to accommodate pedestrian, bicycle and Neighborhood Electric Vehicles.

Design Parameters	Min.	Max.
Target Speed	15	25
Travel Way Dimensions	Min.	Max.
Travel Lane(s) Width (ft)	12	17
Parking Lane Width (Parallel Parking) (ft)	8	9
Parking Lane Width (Angled Parking) (ft)	18	21
Public Frontage	Min.	Max.
Planting Type	Intermittent	
Planting Width (ft)	0	6
Walkway Width (ft)	10	-
Right Of Way	Min.	Max.
ROW Width	120	200

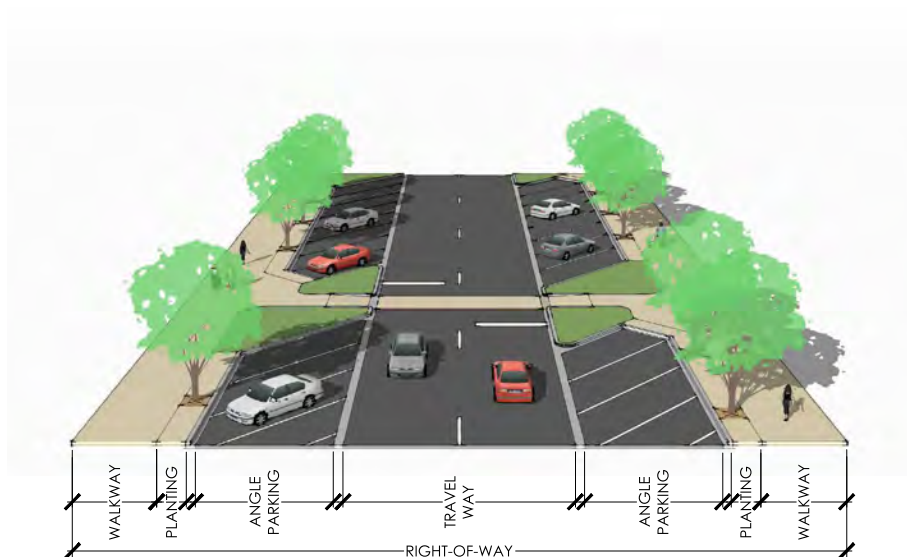


Figure 3.7.2.A.

Mixed Use Street A

Mixed-Use Streets are generally free movement local roadways that provide on-street parking for higher density areas, including shops, offices, multi-family buildings, and Townhouses.

- Free movement local roadway providing on-street parking for higher intensity uses including shops, offices, multi-family buildings, and Townhouses.
- Typically a two-lane undivided roadway.
- May be designed for 25 mile per hour speed limit.
- Designed for on-street parking (angled or parallel).
- Public frontages may include landscaped planters with vegetation.
- Designed to accommodate pedestrian, bicycle connectivity, and Neighborhood Electric Vehicles.

Design Parameters	Min.	Max.
Target Speed	15	25
Travel Way Dimensions	Min.	Max.
Travel Lane(s) Width (ft)	11	12
Bike Lane Width (ft)	5	7 (Buffered)
Optional Parking Lane Width (Parallel Parking) (ft)	8	9
Parking Lane Width (Angled Parking) (ft)	N/A	N/A
Public Frontage	Min.	Max.
Planting Type		Intermittent
Planting Width (ft)	-	6
Walkway Width (ft)	5	-
Right Of Way	Min.	Max.
ROW Width	60	90

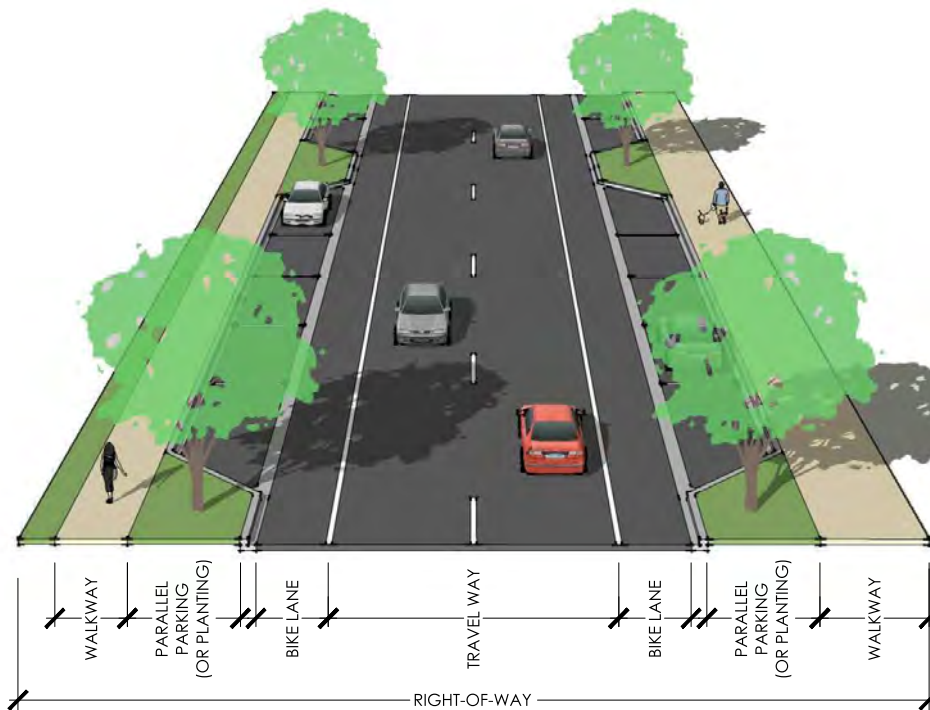


Figure 3.7.2.B.

Mixed Use Street B

Mixed-Use Streets are generally free movement local roadways that provide on-street parking for higher density areas, including shops, offices, multi-family buildings, and Townhouses.

- Free movement local roadway.
- May provide frontage and on-street parking for higher density uses including shops, offices, multi-family buildings, and Townhouses.
- Typically a two-lane undivided roadway.
- May accommodate parallel parking.
- May be designed for 25 mile per hour speed limit.
- Designed to accommodate pedestrian connectivity with sidewalks, separated from vehicular traffic with landscaping, as well as bicycles and Neighborhood Electric Vehicles.

Design Parameters	Min.	Max.
Target Speed	15	25
Travel Way Dimensions	Min.	Max.
Travel Lane(s) Width (ft)	11	12
Bike Lane Width (ft)	N/A	N/A
Optional Parking Lane Width (Parallel Parking) (ft)	8	9
Optional Parking Lane Width (Angled Parking) (ft)	N/A	N/A
Public Frontage	Min.	Max.
Planting Type	Intermittent	
Planting Width (ft)	4	-
Walkway Width (ft)	5	-
Right Of Way	Min.	Max.
ROW Width	54	80

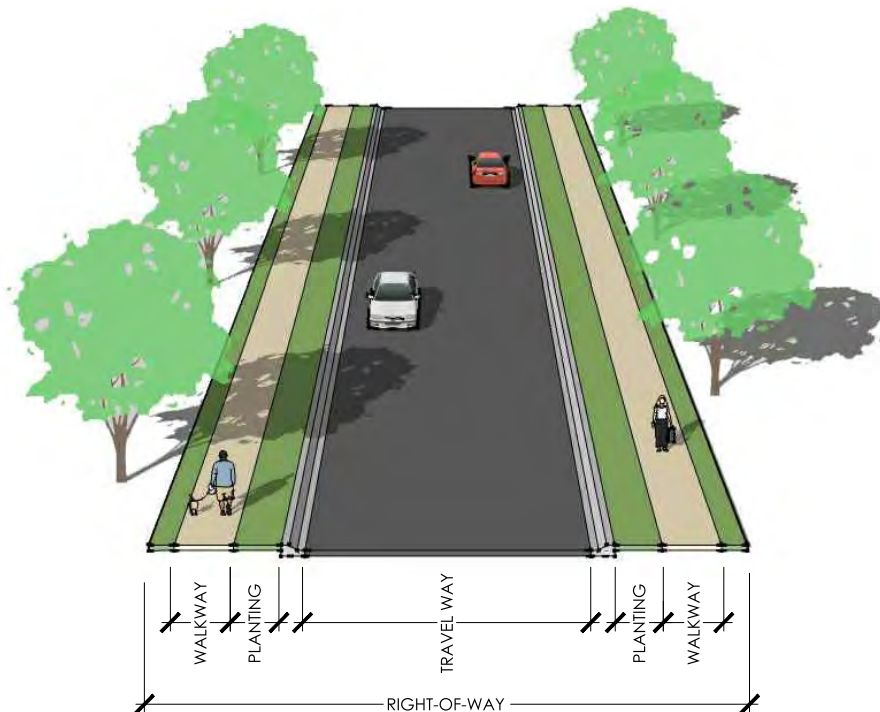


Figure 3.7.3.A.

Avenue

Avenues are frequently used roadways, intended to connect neighborhoods to one another. They are typically free movement local roadways that provide circulation and connectivity to other areas of the community. Avenues are intended to divert higher traffic volumes away from residential neighborhoods while allowing for interconnectivity within the Village.

- Frequently used roadways, intended to connect neighborhoods to one another.
- Typically, a two-lane undivided roadway.
- Designed to accommodate pedestrian, bicycle and Neighborhood Electric Vehicles (may be a trail).
- May be designed for 35 mile per hour speed limit.
- May be lined with canopy trees.

Design Parameters	Min.	Max.
Target Speed	25	35
Travel Way Dimensions	Min.	Max.
Travel Lane(s) Width (ft)	11	12
Optional Bike Lane Width (ft)	5	7 (Buffered)
Parking Lane Width (Parallel Parking) (ft)	N/A	N/A
Parking Lane Width (Angled Parking) (ft)	N/A	N/A
Public Frontage	Min.	Max.
Planting Type	Planted	
Planting Width (ft)	4	-
Optional Walkway Width (ft)	5	-
Multi-Modal Trail Width (ft)	8	12
Right Of Way	Min.	Max.
ROW Width	60	100

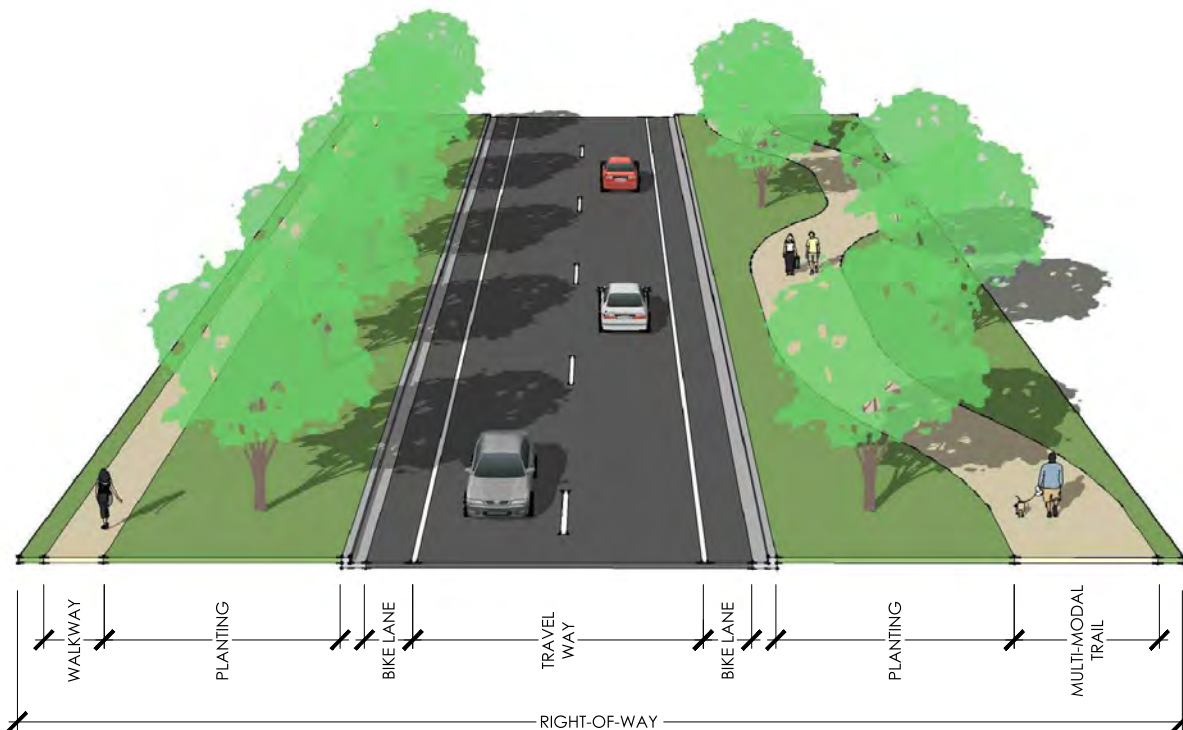


Figure 3.7.3.B.

Avenue At Intersection

Avenues are frequently used roadways, intended to connect neighborhoods to one another. They are typically free movement local roadways that provide circulation and connectivity to other areas of the community. Avenues are intended to divert higher traffic volumes away from residential neighborhoods while allowing for interconnectivity within the Village.

- Frequently used roadways, intended to connect neighborhoods to one another.
- Typically a two-lane undivided roadway.
- Designed to accommodate pedestrian, bicycle and Neighborhood Electric Vehicles (may be a trail).
- May be designed for 35 mile per hour speed limit.
- Lined with canopy trees.
- Turn lanes are included to accommodate safe turning movements.

Design Parameters	Min.	Max.
Target Speed	25	35
Travel Way Dimensions	Min.	Max.
Travel Lane(s) Width (ft)	11	12
Turn Lane Width (ft)	10	12
Optional Bike Lane Width (ft)	5	7 (buffered)
Parking Lane Width (Parallel Parking) (ft)	N/A	N/A
Parking Lane Width (Angled Parking) (ft)	N/A	N/A
Public Frontage	Min.	Max.
Planting Type	Planted	
Planting Width (ft)	4	-
Optional Walkway Width (ft)	5	-
Multi-Modal Trail Width (ft)	8	12
Right Of Way	Min.	Max.
ROW Width	60	120

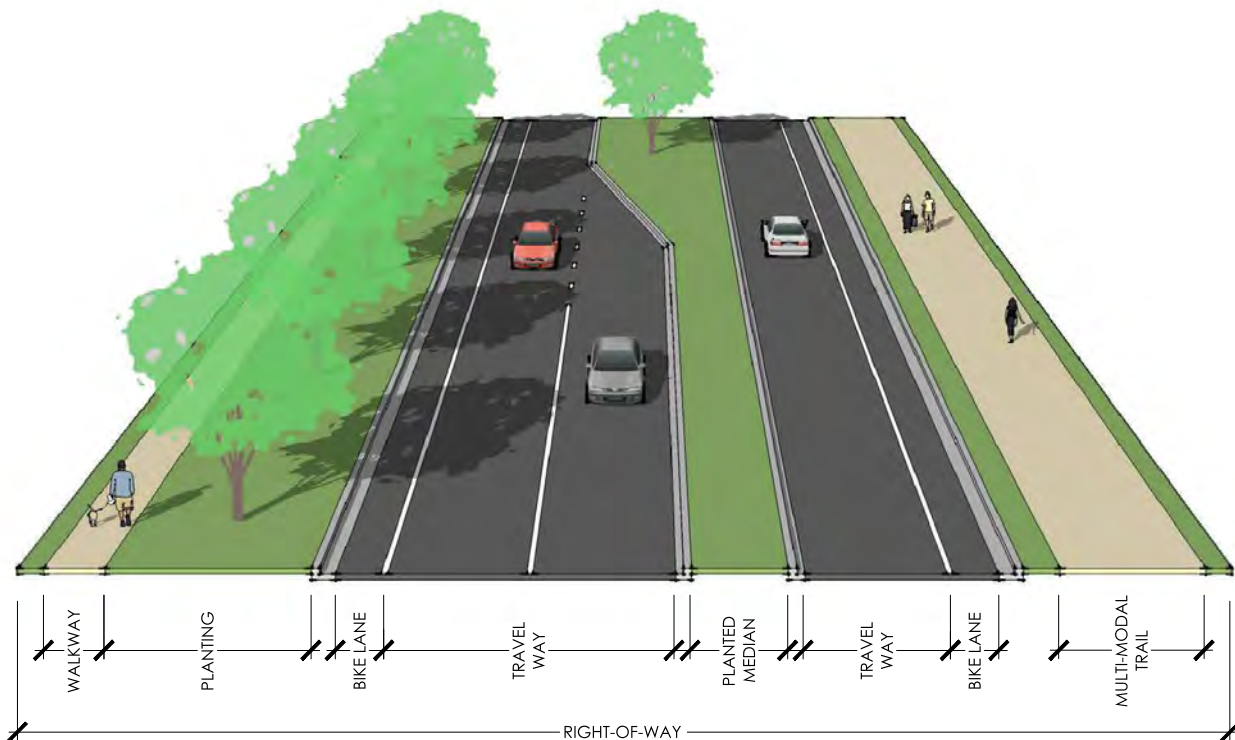


Figure 3.7.4.A.

Residential Neighborhood Street

Residential Neighborhood Streets are small scale, slow movement local roadways suitable for neighborhood areas.

- Small scale, slow movement local roadways suitable for neighborhoods.
- Typically two lanes, undivided.
- Link neighborhoods and neighborhood centers to avenues.
- May be designed for speed limits of up to 25 miles per hour, typically posted lower.
- Accommodates neighborhood vehicles, pedestrians, bicycles and Neighborhood electric vehicles.

Design Parameters	Min.	Max.
Target Speed	15	25
Travel Way Dimensions	Min.	Max.
Travel Lane(s) Width (ft)	10	12
Turn Lane Width (ft)	N/A	N/A
Bike Lane Width (ft)	N/A	N/A
Parking Lane Width (Parallel Parking) (ft)	N/A	N/A
Parking Lane Width (Angled Parking) (ft)	N/A	N/A
Public Frontage	Min.	Max.
Planting Type	Planted	
Planting Width (ft)	4	-
Walkway Width (ft)	5	-
Right Of Way	Min.	Max.
ROW Width	40	60



NOTE: Sidewalk only required along frontage of residential units.

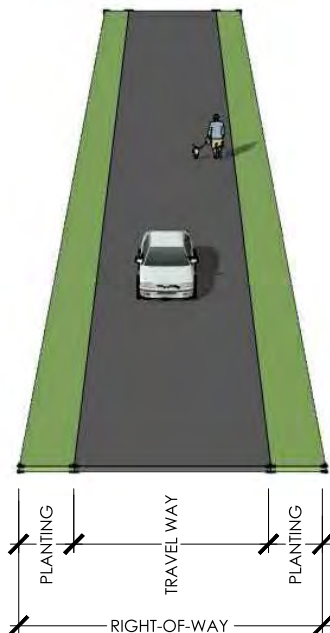
Figure 3.7.5.A.

Residential Neighborhood Alley

Residential Neighborhood Alleys are small scale, slow movement drives for rear access to homes.

- Provide access to rear loading garages or parking areas.
- Designed with 10 foot travel lanes.
- Designed as one-way sections.
- Intended for “traditional neighborhood designs” in neighborhoods.
- Intended to provide access to rear of commercial/mixed use buildings in Mixed Use areas and Mixed Use Residential Neighborhood.

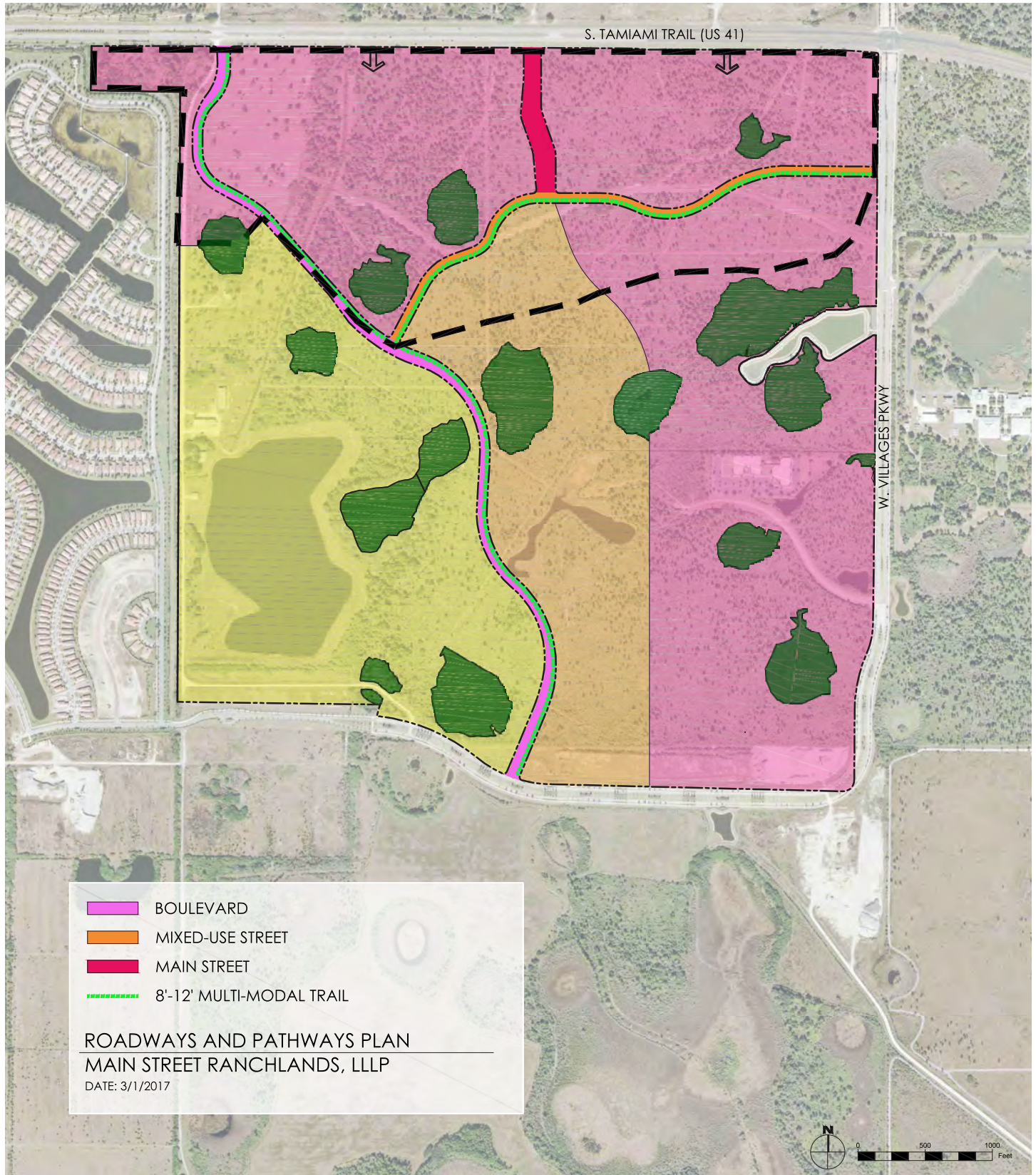
Design Parameters	Min.	Max.
Target Speed	N/A	N/A
Travel Way Dimensions	Min.	Max.
Travel Lane(s) Width (ft)	10	16
Turn Lane Width (ft)	N/A	N/A
Bike Lane Width (ft)	N/A	N/A
Parking Lane Width (Parallel Parking) (ft)	N/A	N/A
Parking Lane Width (Angled Parking) (ft)	N/A	N/A
Public Frontage	Min.	Max.
Planting Type	N/A	N/A
Planting Width (ft)	N/A	N/A
Walkway Width (ft)	N/A	N/A
Right Of Way	Min.	Max.
ROW Width	15	25



3.7.6.A

Multi-Modal Trails

Multi-Modal Trails are designed in designated rights-of-way. They are intended to be 8 to 12 foot trails that blend with surrounding neighborhoods and neighborhood centers through the use of landscaping and native vegetation and trees that are consistent with surrounding areas. Finally, the surface of the trail may vary from paved to other materials (i.e. mulch, shell, etc.).



Proposed Infrastructure Plan

3.8 Infrastructure

Preliminary provisions have been made for water, wastewater, stormwater and solid waste as required by the City of North Port Unified Land Development Code. The West Villages are already subject to the Principles of Agreement addressing the provision of major infrastructure to serve the existing and proposed villages. The Principles of Agreement and Utility Agreement serve to meet the Developers Agreement requirements pursuant to Chapter 54 ULDC.

Water, sewer, reuse and irrigation infrastructure is available to the property. Formal negotiations are underway to ensure availability concurrent with development through mains in adjacent roadways. The agreement to accommodate long- and short-term service was formally initiated through a memorandum dated September 18, 2000 regarding water and sewer availability for the West Villages. The City's memorandum includes the anticipated responsibilities of both the City and the West Villages Improvement District for providing utility service during interim and final build out periods for the area. At present, the City has water and sewer infrastructure capacity to serve the subject site; however, additional utility infrastructure is anticipated, and includes water and wastewater treatment infrastructure, intended to provide service to this and other Villages. The West Villages Improvement District (WVID) was formed to provide a mechanism to construct the utility infrastructure required to serve new development within the West Villages.

3.8.1 Water and Wastewater

At this time, water and sewer infrastructure sufficient to serve portions of Village D will be provided through an existing water main and force main that connect to existing City plants. A potable water pump station and storage tank has been constructed to ensure adequate pressure during peak domestic and fire demand.

It is anticipated water for irrigation will be supplied through wells and surface waters as permitted by water use permits and may be supplemented by reclaimed water. The long-term service requirements of the West Villages are planned to be served with centralized water and sewer systems, as well as a dispersed irrigation quality water system. It is anticipated that new water and wastewater treatment infrastructure will be co-located and constructed within the West Villages. The WVID will be required to design and construct water and wastewater treatment infrastructure, then ultimately turn them over to the City to own and operate.

3.8.2 Stormwater Management

Stormwater will be retained in a system of lakes within the village. Ideally, the lakes will serve the village as a whole, including the individual areas and neighborhoods. The stormwater lakes are being sized to effectively accommodate stormwater demand for residential and non-residential uses. These features will serve as a community amenity. Specific lake size and topographic alterations will be developed as part of the next steps of the Village District planning process and construction plan development.

The VDPB permits the consideration of Low Impact Design (LID) strategies to address stormwater management requirements in the West Villages.

3.8.3 Solid Waste

Solid waste is expected to be collected by the City of North Port Solid Waste District. Preliminary plans allow collection vehicles to enter the community and collect waste either from dumpster enclosures for non-residential or mixed use buildings, and from individual units in neighborhoods. The City of North Port has not identified any deficiencies in solid waste capacity.

Figure 3.8.A.
Water System Plan

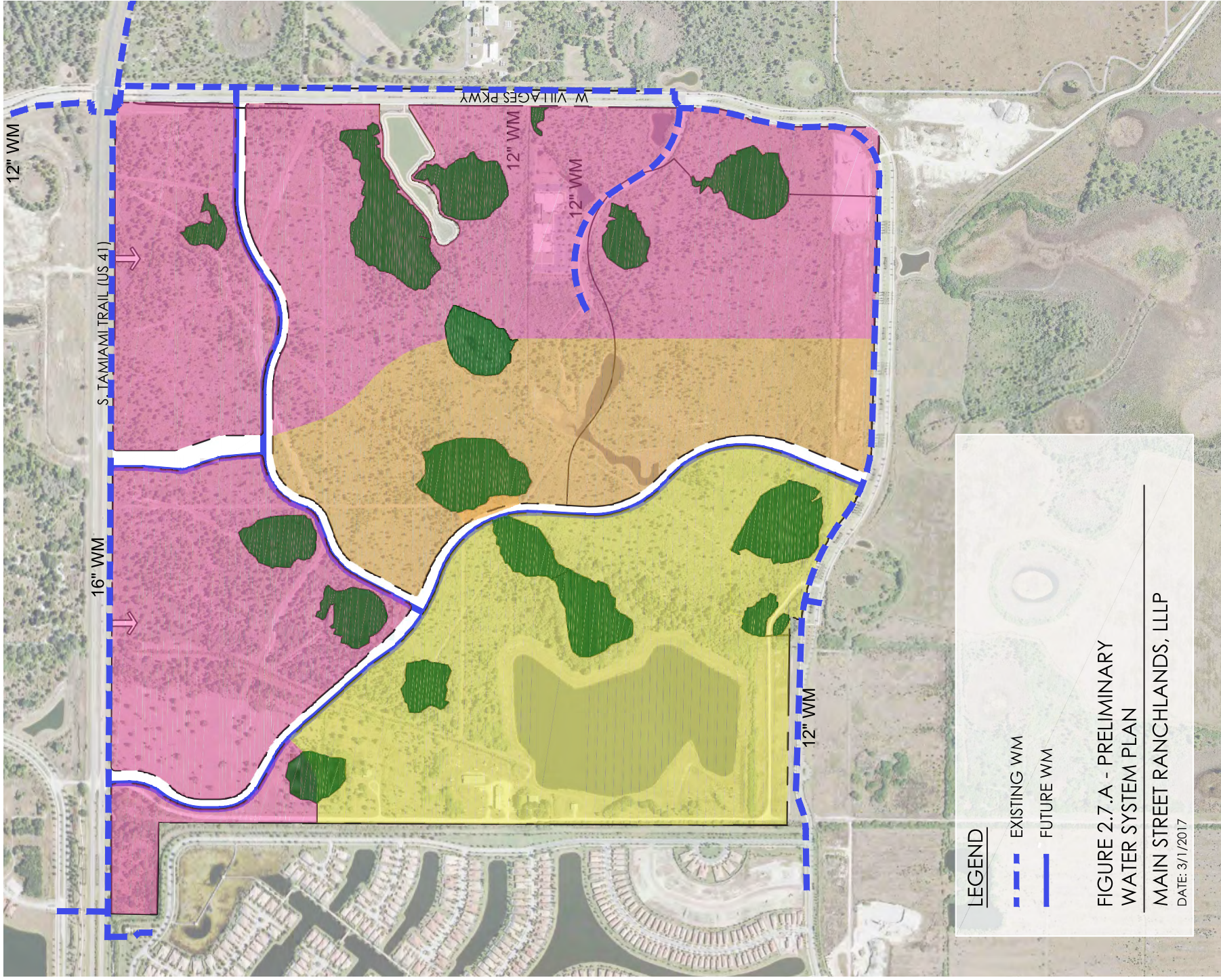
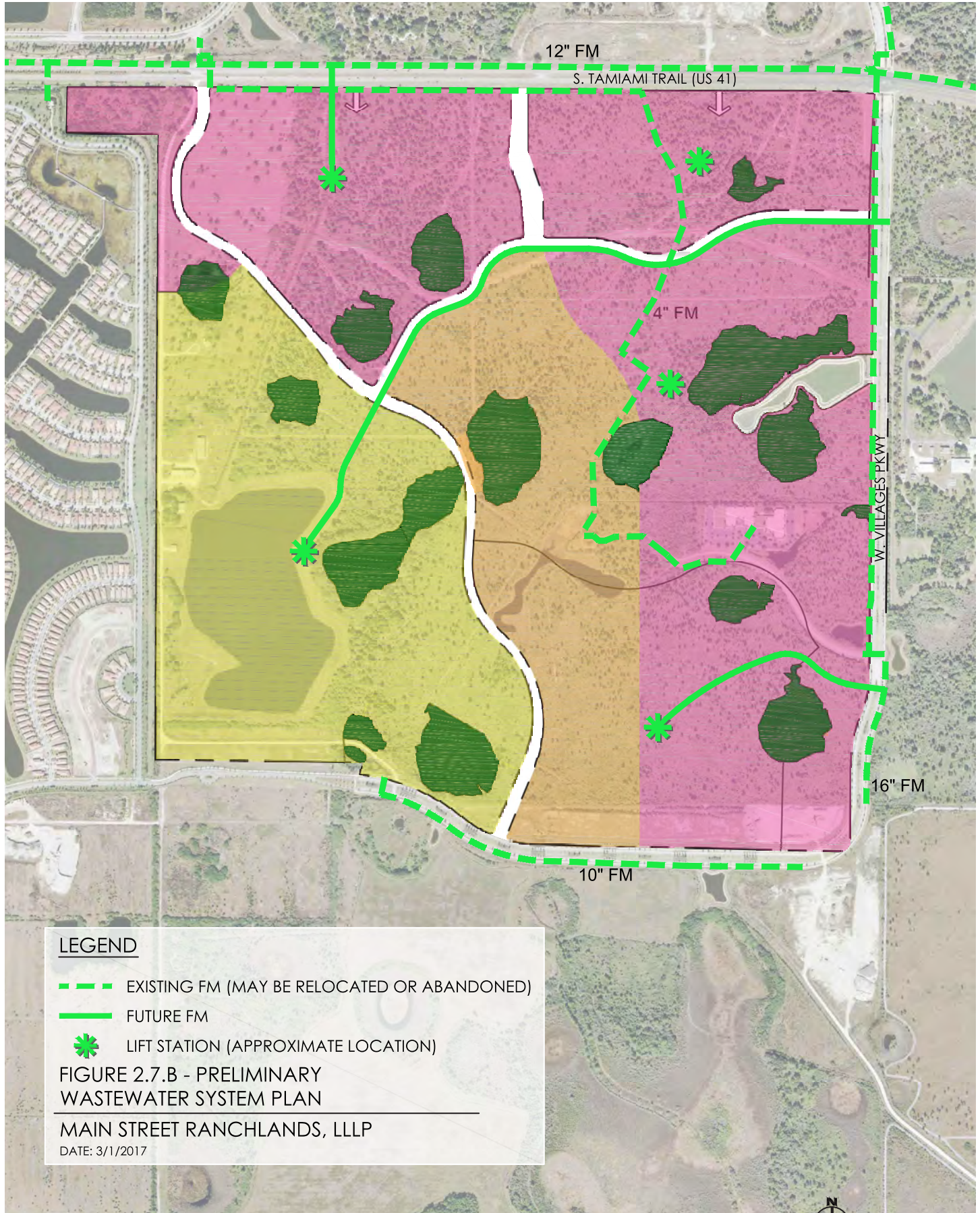


Figure 3.8.B.

Wastewater Systems Plan



Environmental Management Plan

3.9 Environmental Overview

ECT has identified areas of native habitats and listed species that have potential to be impacted by the Proposed Village District Pattern Plan (VDPP) as required under Section 53-214(C)(9)(c)(iii) of the City of North Port (CONP) Unified Land Development Code (ULDC). Native habitats occurring within West Villages Town Center were previously described in the Environmental Site Analysis prepared by ECT in April 2016. The project contains a total of 514.2 acres of uplands and 112.2 acres of wetlands and surface waters. As noted in the Environmental Site Analysis, the jurisdictional extent of wetlands and surface waters were previously verified and approved by the Southwest Florida Water Management District (SWFWMD) pursuant to Formal Determination of Wetlands and Other Surface Waters Petition No. 42032522.000 issued on 16 August 2007. This binding jurisdictional determination expired in August 2012. Therefore, SWFWMD will reevaluate wetland jurisdiction as part of future permitting efforts for development.

The majority of the site consists of pine flatwoods, but other upland habitats (i.e., hardwood-conifer mixed) occur onsite along with a number of other land uses that support the agricultural operations (i.e., improved pasture, jeep trails) and the old hospital facility which is now being used as an office space. The site also contains a number of wetlands and surface waters characterized mostly by freshwater marsh systems, ditches and swales, a borrow pit and stormwater ponds. Areas of native habitat that have potential to be impacted by the Proposed VDPP are summarized below along with state and federally listed species occupying these habitats.

3.9.1 Impacts to Native Habitats

As depicted on the Environmental Impact Plan (Figure 3.9.A), the majority of development will occur in uplands; however, some wetlands and surface waters will also be impacted by the proposed development. Areas of native habitat that will be impacted by the Proposed VDPP are discussed below for both upland and wetland habitats including surface waters.

Upland Habitats

In an effort to avoid and minimize impacts to wetlands, the majority of development will occur within uplands. The uplands are primarily characterized as pine flatwoods (FLUCFCS 411); therefore, these areas will be most affected by development.

Other areas of native upland habitat that will be affected include hardwood-conifer mixed communities (FLUCFCS 434). Areas of native habitat that will be preserved include portions of pine flatwoods and hardwood-conifer mixed communities that will be incorporated into the wetland buffers, as well as any areas that are incorporated into greenbelts or open space areas, the locations of which have not been defined. Furthermore, native upland habitat will ultimately be preserved within the protection zones surrounding an existing eagle nest which is discussed in further detail below.

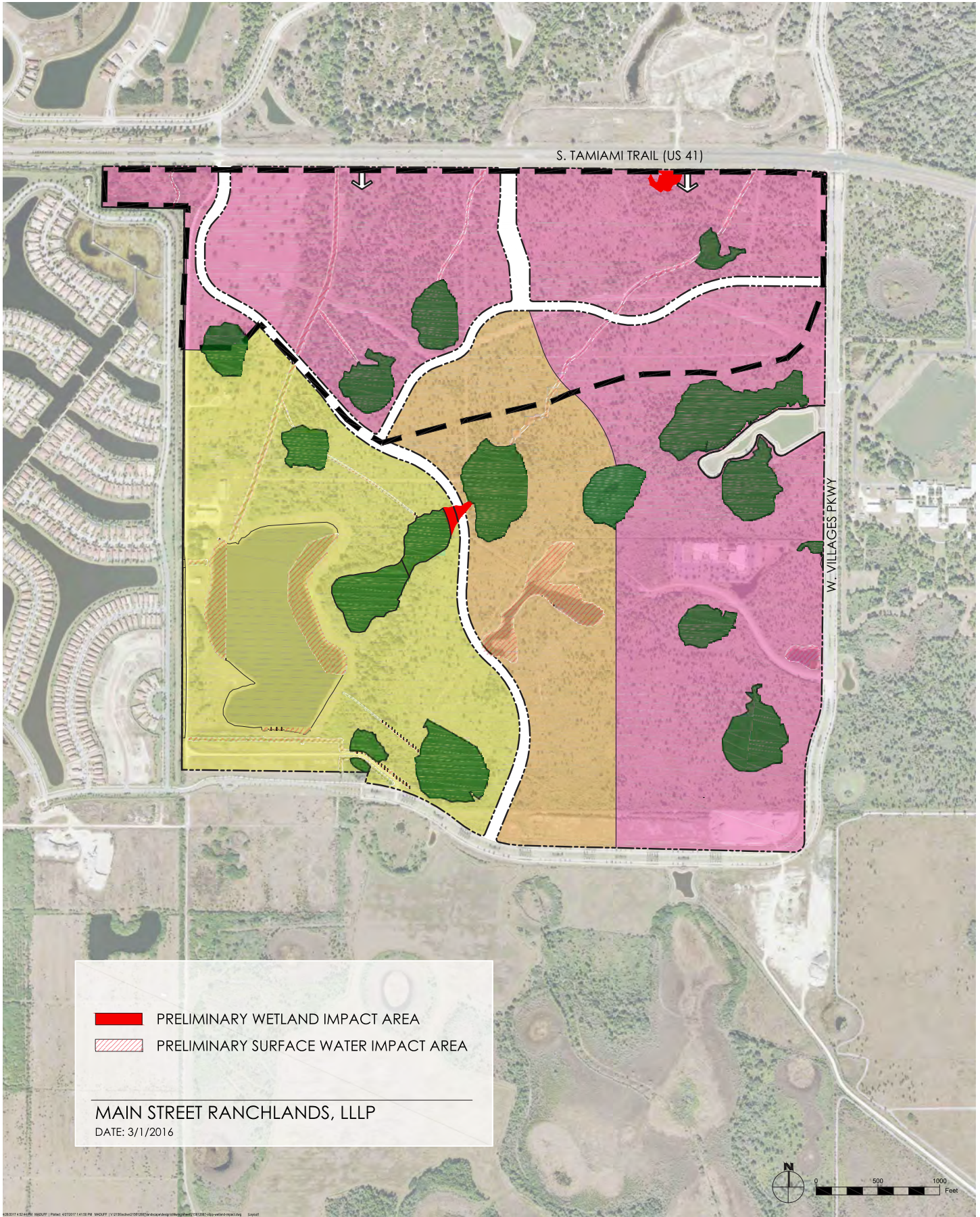
Wetlands and Surface Waters

As shown on the Environmental Impact Plan (Figure 3.9.A), the majority of wetlands are being avoided with the exception of two wetlands (1.19 acres) where the future alignment of roads will necessitate impacts. Wetland impacts shown on the Environmental Impact Plan may be refined during the SWFWMD permitting process. When no other reasonable alternative exists to avoid wetland impacts, mitigation will be provided consistent with state and federal requirements to ensure no net loss of wetland and surface water functions. Mitigation may be provided in the form of enhancement, restoration, creation or preservation, where appropriate.

For all preserved wetlands, buffers will be maintained around remaining wetlands to avoid secondary wetland impacts consistent with Section 10.2.7 of the Environmental Resource Permit (ERP) Applicant's Handbook, Volume 1. Minor buffer encroachments may be necessary in some cases to accommodate roads, stormwater infrastructure and rear grading of lots, but this will be revisited when detailed site plans are developed in support of future permitting.

In addition to wetlands, approximately 15.9 acres of surface waters may be impacted to accommodate future development. These surface waters include ditches (FLUCFCS 513), swales (FLUCFCS 514) and a large borrow pit (FLUCFCS 526), all of which are man-made. These surface waters are not considered native habitat because of the fact that they are man-made, but are still regulated by the State. The existing stormwater ponds (FLUCFCS 527) that support the old hospital complex may also be impacted or reconfigured, but these ponds are not considered jurisdictional surface waters since they are permitted treatment facilities. With the exception of the larger ditches and borrow pit, the majority of these surface waters provide minimal habitat value for fish and wildlife; however, new habitat will be provided by stormwater ponds to replace any habitat loss associated with surface water impacts.

Figure 3.9.A.
Wetland Impact Plan



Section 3.9.2 Impacts to Listed Species

ECT conducted a preliminary listed species survey to determine the potential for state or federally listed species to occur onsite. As outlined in the Environmental Site Analysis (April 2016), ECT observed a total of four (4) listed species onsite including gopher tortoises (*Gopherus polyphemus*), Southeastern American kestrels (*Falco sparverius paulus*), bald eagles (*Haliaeetus leucocephalus*) and wood storks (*Mycteria americana*). No other listed species were directly observed, but certain species are recognized as having potential to occupy certain habitats onsite. Below is a summary of potential impacts the Proposed VDPP will have on listed species.

Gopher Tortoises

Gopher tortoise burrows have been observed onsite and are expected to occur given the abundance of upland habitats. Prior to construction, a 100% gopher tortoise survey will be required in accordance with Florida Fish and Wildlife Conservation Commission's (FWC) Gopher Tortoise Permitting Guidelines (Revised February 2015) to locate all tortoises and their burrows within the development footprint. Once all tortoises (and their burrows) are located, appropriate authorization will be obtained from FWC to relocate tortoises to an approved recipient site.

Gopher Frogs

Gopher frogs (*Rana capito aesopus*) are a commensal species that are often found occupying gopher tortoise burrows near ephemeral ponds and wetlands, which are used for breeding grounds. No gopher frogs or indications of their presence were observed during the preliminary survey, but the potential presence of the species will be investigated further during the 100% gopher tortoise survey.

Eastern Indigo Snakes

No Eastern indigo snakes (*Drymarchon corais couperi*) or indications of their presence were observed during the preliminary survey, but their cryptic nature can make them difficult to detect. Eastern indigo snakes have potential to occur onsite as they can be found in nearly any wooded habitat throughout Florida including pine flatwoods, hardwood forests and hammocks. They are also a commensal species commonly associated with gopher tortoise burrows. The potential for this species to occur onsite will be investigated further during the 100% gopher tortoise survey. Regardless of whether this species is observed, the Applicant will be required to follow the U.S. Fish and Wildlife Service's (USFWS) Standard Protection Measures for the Eastern Indigo Snake (August 2013) to minimize potential conflicts with this species during construction.

Sherman Fox Squirrel

As noted in the Environmental Site Analysis (April 2016), conditions of the onsite habitats are not suitable for Sherman's fox squirrels (*Sciurus niger shermani*). They prefer a more open understory as opposed to the highly overgrown, fire-suppressed habitats found onsite. Therefore, no adverse impacts are anticipated for fox squirrels.

Listed Bird Species

ECT also evaluated potential impacts for listed bird species including bald eagles, Florida scrub-jays (*Aphelocoma coerulescens*), Southeastern American kestrels and a number of listed wading birds. Based on the FWC eagle nest database, the closest bald eagle nest is located approximately 4 miles northwest of the project site. However, ECT observed bald eagles nesting onsite for a period of 2-3 weeks towards the end of the nesting season (mid-February to early March 2016) in which case the nest was abandoned. No hatchlings were observed likely due to the late nesting attempt. The location of this nest is depicted on the Listed Species Map (Figure 4) provided in the Environmental Site Analysis (April 2016).

ECT has since observed eagles actively nesting in this same location starting in November 2016. The eagles are still actively nesting. Therefore, mandatory eagle nest protection zones (330 to 660 feet) will be maintained in accordance with FWC's Bald Eagle Management Guidelines (April 2008) and the USFWS's National Bald Eagle Management Guidelines (May 2007). Any activities proposed within the protection zones will be subject approval from FWC and USFWS.

No Florida scrub-jays were observed nor does the site contain any suitable scrub habitat to support this species. ECT verified Sarasota County's database for known scrub-jay territories which shows potential for scrub jays to occur on lands east of West Villages Parkway. However, this is based on data that is over ten years old. More comprehensive wildlife surveys will be conducted as part of future development applications in which case the need for a formal scrub jay survey will be evaluated through coordination with USFWS. Based on recent surveys, the project is not anticipated to adversely affect Florida scrub-jays.

As outlined in the Environmental Site Analysis (April 2016), ETC observed kestrels perched on the powerline that runs along the main ranch road on the west side of the property. However, the kestrels were observed during the winter season when the northern subspecies (*Falco sparverius*) migrate to Florida. The northern subspecies is not protected. Therefore, kestrel surveys will need to be conducted outside of the

migratory season to confirm whether the kestrels are the protected species. No evidence of nesting was observed.

ECT evaluated the site for state and federally listed wading birds given the occurrence of wetlands and surface waters onsite. A number of wading birds have been observed onsite including wood storks which are federally protected. No evidence of nesting was observed but wading birds are expected to use the wetlands and surface waters for foraging opportunities. Any loss in wading bird habitat associated with wetland or surface water impacts would need to be appropriately mitigated through wetland mitigation or other means of habitat creation (i.e., stormwater ponds, floodplain compensation areas). Furthermore, any impacts to wetlands and surface waters that are considered suitable foraging habitat for wood storks could be subject to review by USFWS. This may require a wood stork foraging impact analysis to ensure no net loss of wood stork habitat which consequently would address all wading birds. Therefore, future development of this site is not expected to have a negative impact on wading bird habitat.

Public Facilities Plan

3.10.1 Roadways

Comprehensive Plan Policy 13.6 requires that each Village District Pattern Plan include an evaluation of the public facilities needed to support the development. A Transportation Impact Analysis for Village D has been completed to predict the impacts of Village D on the transportation system, and to identify improvements. The traffic impacts are based on the final village plan and buildout year of 2030.

The study area was based on the total buildout of the site and consists of arterial and collector roads where project traffic equals or exceeds five percent of the adopted two-way peak-hour service volume, as specified by the 2012 FDOT Generalized Service Volume Tables or Sarasota County's 2015 Generalized Level of Service Tables. The study area also includes collector and arterial roads to which the project has direct access or that the project accesses via a private or local roads.

Per the criteria, 20 segments meet or exceed the five percent significance threshold. In addition to the roadway segments evaluated in the study area, intersections of regulated roadways within the study area were evaluated. The 21 regulated roadway intersections that were studied are listed below.

1. River Rd/I-75 NB
2. River Rd/I-75 SB
3. River Rd/Venice Ave
4. River Rd/Center Rd
5. River Rd/W Villages Pkwy
6. River Rd/US 41
7. River Rd/East River Rd
8. River Rd/Winchester Rd
9. SR 776/US 41
10. Jacaranda Blvd/US 41
11. Woodmere Park Blvd/US 41
12. Venice E Blvd/US 41
13. Rockley Blvd/US 41
14. W Villages Pkwy/US 41
15. De Miranda Ave/US 41
16. Ortiz Blvd/US 41
17. Biscayne Dr/US 41
18. Pan American Blvd/US 41
19. North Port Blvd/US 41
20. Tuscola Blvd/US 41
21. Sumter Blvd/US 41

The project will utilize two new full median openings to US 41, one new directional median opening to US 41, and a full median opening to West Villages Parkway for project access. The three new median openings proposed as part of the development on US 41 have been discussed with the Florida Department of Transportation (FDOT) Access Management staff and preliminarily approved. Additional right-in/right out connections to US 41 and West Villages Parkway may be constructed for access to the commercial and/or office properties, but were not evaluated as part of the concurrency analysis. If/when they are constructed; they will be evaluated as part of the parcel specific development plan review to ensure they meet FDOT and West Villages Improvement District access management standards.

A transportation concurrency analysis and site access analysis were both performed. The concurrency analysis was performed for the PM peak-hour. The transportation concurrency analysis identified roadway and intersection deficiencies in the existing and 2025 background traffic conditions. Pursuant to Florida Statutes, the necessary improvements to correct preexisting deficiencies shall be considered in place. A developer is only responsible for correcting those transportation deficiencies that are directly created by the addition of their project traffic. The Phase 1 project traffic will not create any additional deficiencies.

The 2030 background traffic analysis identified only intersection deficiencies. When the remainder of the project traffic (Phase 2) is added to the 2030 background traffic conditions, a deficiency is created at the River Road/US 41

intersection. The following intersection improvements are required with the addition of the project traffic in 2030 and are subject to the developer paying Proportionate Fair Share. The estimated Proportionate Fair Share is \$273,000.

- River Road/US 41: Construct an 800-foot northbound left turn lane

The site access analysis evaluated the four driveway connections creating new median openings. The following site access improvements are required in conjunctions with this project. All turn lanes should be constructed in accordance with the FDOT Plans Preparation Manual and Standard Index 301.

Driveway 1/US 41

- Signalize when warranted or construct a roundabout
- Construct a 350-foot eastbound right turn lane
- Construct a 725-foot westbound left turn lane

Driveway 2/US 41

- Signalize when warranted or construct a roundabout
- Construct a 350-foot eastbound right turn lane
- Construct a 650-foot westbound left turn lane

Driveway 3/US 41

- Construct a 350-foot eastbound right turn lane
- Construct a 450-foot westbound left turn lane

West Villages Parkway/US 41

- Construct a 150-foot southbound right turn lane
- Construct a 200-foot northbound left turn lane

If intersection improvements are required to support background growth and other future developments in the general vicinity, they may be funded and constructed by public agencies or other developments in the area, or as part of private partnerships between development entities.

It is important to note that residential and commercial uses will be assessed a transportation impact fee. At time of build-out, the Village is expected to generate approximately \$4.9 million in impact fees, which will fund future transportation improvements in the area.

3.10.2 Schools

Residential development contemplated for Village D has the potential to add school aged children to the population of West Villages. This property is primarily located in the Taylor Ranch Elementary, Venice Middle School, and Venice High School attendance zones, among others.

In consideration of the long-range development of West Villages, and the demands that may place on existing schools, the owners of West Villages entered into a Real Property Dedication Agreement with the School District of Sarasota County on October 6, 2016. This agreement memorializes the owners' commitment to convey 60+/- acres of property, having frontage on West Villages Parkway, and generally located east of Village "D".

The School Board is entitled, but not obligated to construct Educational Facilities on the donated land, and may construct an elementary school, middle school, high school or any combination of such schools as a shared campus. In exchange, the owners are entitled to Educational System Impact Fee Credits, as described in the Agreement.

School concurrency review will occur at time of plat, plan, or functional equivalent, in accordance with the requirements of the Sarasota County School Board Policy, the Interlocal Agreement for Public School Facility Planning (as amended), and the City of North Port Comprehensive Plan, as applicable.

It is anticipated that school impact fees, that can be either collected, or credited, consistent with the Real Property Dedication Agreement, throughout the development of Village "D", could result in up to \$2.49 million in revenue for future school facility needs.

3.10.3 Fire and Police Protection

The proposed village design will include sufficient water supply lines and infrastructure to provide the required fire flows and pressures. Fire hydrants will be appropriately located and readily available for fire protection. Fire protection impact fees will also be assessed at time of Certificate of Occupancy. At time of buildout, the village is projected to have contributed up to \$453,179 in fire protection impact fees.

The City of North Port and Sarasota County currently have an inter-local agreement for County fire services. Sarasota County Fire Station #26 is located adjacent to the State College of Florida campus, east of this village, and accessed from US 41. The City also provides services from its Station #2, located on North Port Boulevard at City Hall. Normal protocol for Firefighters/EMTs is to respond to emergencies, regardless of political boundaries.

Residential neighborhoods within the Village are anticipated to have gated entrances or other security features. These elements are expected to minimize demand for police presence. The village is projected to generate up to \$429,750 in law enforcement impact fees at buildout.

The City's Police Department is currently headquartered on City Hall Boulevard next to City Hall. Normal protocol for Officers involves the continuous patrolling of various sections of the City while concurrently dispatched to emergency calls.

3.10.4 Transit

Public bus lines serve this area of Sarasota County; however ridership is relatively low. Some possible reasons for this low ridership could be a result of limited population densities in the area, as well as limited pedestrian oriented design in this part of the City. The final village plan for Village D includes pedestrian linkages and multi-modal trails that will encourage alternative forms of transportation. Given the proposed design of Village D, use of transit is expected to be more feasible. However, it is important to note that transit use is typically relatively low in this less urbanized area.

The Sarasota County Area Transit (SCAT) has two fixed-routes, #9 and #90X, that travels US 41, linking the City of North Port to the City of Venice, where riders may transfer to buses that reach the City of Sarasota. Route #9 begins service at the intersection of US 41 and Sumter Boulevard and takes approximately 55 minutes to reach the intersection of East Tampa and US 41 Business. State College of Florida, south of US 41, and east of Village D, has one transit stop on route #9. Route #90X is an express route that begins service at the North Port City Hall, travels through Venice and Sarasota and ends at Sarasota Bradenton International Airport.

Future transit routes and stops within Village D will be coordinated with SCAT with future thoroughfare roadway design. At a minimum, design for transit stops and/or bus shelters will follow the design criteria established by SCAT.

3.10.5 Hurricane Evacuation

In the event of a hurricane evacuation, Village D residents will use I-75 as the major evacuation route to leave the area. The interstate can be accessed from both River Road and Jacaranda Boulevard, located east and west of Village D, respectively. Both roads link to I-75, from US 41.

River Road is considered the "Englewood Interstate Connector (EIC)", to improve hurricane evacuation capability. Given this, Sarasota County designed, and plans to construct a six-lane improvement project for River Road from US 41 North, to Center Road, and four lanes from Center Road to I-75.

Jacaranda Boulevard is currently a four-lane road from US 41 to I-75. It is designated as a six-lane road from Center Road north to I-75, but widening is not programmed at this time. Both Jacaranda Boulevard and River Road intersect with

US 41. Residents evacuating Village D will travel either east or west on US 41 to River Road and Jacaranda Boulevard, respectively. From there, residents will travel north to I-75. As an alternative, residents may travel northeast on West Villages Parkway, to River Road, then north to I-75.

3.10.6 Solid Waste

Future residents and businesses of Village D, will receive waste services from the City of North Port Solid Waste Division. All residential real property that has a certificate of occupancy is required to pay a non-ad valorem assessment, which is currently \$249.00 per fiscal year. Costs associated with commercial development depends on the type of trash collection facilities needed for a specific commercial development (i.e. containers, dumpsters or compactors).

It is important to note that this project is part of the WVID and will be subject to the agreement reached between the City and WVID, as it relates to proportionate share of cost of services.

Village Economic Impact Analysis

3.11 Village Economic Impact Analysis

This planned village is expected to have a positive economic impact on the City of North Port. The many public improvement projects required for the planned village will be funded in part by the village developers, future tax assessments, and the West Villages Improvement District. Details regarding the anticipated public improvements and anticipated public services are identified in Section 3.11 of this document. This portion of the Proposed VDPP will address revenues associated with Village development.

3.11.1 Improvement and Maintenance Entities

In general, the majority of future services will be provided by the City of North Port. The City will be the sole provider for water, sewer; police, fire, planning and zoning, and solid waste. Capital improvements intended to serve the West Villages will be facilitated by the West Villages Improvement District (WVID). The village developers will also construct and fund many initial improvements, however, the West Villages Improvement District will oversee long-term maintenance and capital improvement projects. The Improvement District will fund and maintain roads, security, landscaping, and utility infrastructure that directly benefits the communities in the West Villages. Other functions like police, fire, schools, transit, and libraries, will be administered by other governmental agencies and operated using ad valorem tax revenues generated by development in the village.

The WVID will fund, initiate and maintain various improvements that will benefit property within the West Villages. Specifically, the WVID initiated, funded, and maintains West Villages Parkway and will do the same for other public roads within the West Villages, as well as utility infrastructure throughout the development. Additionally, the WVID will continue to fund and maintain common areas, such as parks, medians, stormwater retention ponds, and other open space areas.

It is anticipated that water and wastewater facilities will eventually be turned over to the City. All other infrastructure, such as roads, will be maintained by the West Villages Improvement District. Funding for the WVID is provided through special assessments for each dwelling unit and property within the West Villages that receives a direct benefit. These assessments are paid as part of the annual property tax bills.

A land owner, tenant, Property Owner's Association(s) (POA) or a unit of development within the WVID will be created for

Village D as part of its initial construction phases, in order to fund and oversee maintenance within the Village. Generally, elements and services that are reserved solely for Village residents will be funded and managed by a POA while public facilities are maintained by the WVID.

3.11.2 Revenue Generation Estimates

Village D is anticipated to generate three specific revenue sources that will help fund and maintain necessary public services for village residents. The first source is focused on impact fees that will be charged as one-time assessments for each dwelling unit. The fees are intended to mitigate impacts on area roads, fire protection services, libraries, parks and other utility infrastructure. These various impact fees are listed as line items and normally paid at or around the time of Certificate of Occupancy for each unit. Upon buildout, the village is expected to generate up to \$10,679,435.20 in impact fees, which will be used by various governmental agencies. Figure 3.12.A and 3.12.B list the impact fees, as applicable, to residential and commercial development within Village D. Public Utility Capacity Fees for water and sewer are anticipated to generate \$5,632,512.76 at time of buildout. Figure 3.12.C lists the Public Utility Capacity fees that are anticipated for Village D.

The second revenue source is ad valorem and non-ad valorem tax generation. Generally, the average tax rate for property located in North Port is 15.7248 per \$1,000 of assessed property value (including City and other government use taxes). In most cases, an individual property owner is entitled to a \$50,000 homestead exemption that is deducted from the overall assessed value. Based on similar communities in the area within this region, the Village developers have estimated the initial property value for each unit type. In addition, preliminary planning has estimated a finite number of units for each lot type. After applying these estimates, the Village is expected to generate average annual tax revenues of \$8,650,507 on residential development, at time of buildout. This tax generation is expected to increase as part of annual property appreciation in the area. Figure 3.12.D lists the tax revenue calculations, as applicable, to residential development within the Village.

The final revenue source is special assessments that will be assigned by the WVID. These special assessments will be listed as additional line items on property tax bills and collected annually. These assessments are for utilities and roadways. At this time, revenue generated from this assessment is unknown.

Figure 3.11.A. Impact Fees

Figure 3.11.A - Impact Fee - Base Fee

Impact Fee	Single-Family	Multi-Family	Retail (3)	Office (4)
Library (1)	\$333.90	\$323.67	\$0.00	\$0.00
Park	\$558.88	\$329.38	\$0.00	\$0.00
Law Enforcement	\$194.50	\$114.50	\$237.50	\$114.50
Fire Department	\$206.55	\$121.55	\$241.83	\$121.55
Transportation	\$1,928.23	\$1,309.00	\$2,919.33	\$2,093.55
General Government	\$143.65	\$84.50	\$175.50	\$84.50
Solid Waste	\$194.50	\$88.50	\$104.00	\$49.00
School Board (2)	\$2,052.32	\$566.61	\$0.00	\$0.00
SUBTOTAL	\$5,612.53	\$2,937.71	\$3,678.16	\$2,463.10

Notes:

(1) This fee is collected by the City of North Port for Sarasota County and is based on the average fee.

(2) This fee includes Educational Administrative charge.

(3) Retail impact fees based on Shopping Center > 50,000 sfgla (per 1,000 sq. ft.). Actual fees may be based on specific use.

(4) Office impact fees based on Office, General 100,001-200,000 (per 1,000 sq. ft.)

Source: Impact Fee Ordinance 2014-12, City of North Port, Florida; Impact Fees Charged on Development in Unincorporated Sarasota County (updated 8/18/16)

Figure 3.11.B - Impact Fee - Projected Expenditures for Village D

Impact Fee	Single Family	Multi-Family	Retail (3)	Office (4)
Dwelling Unit/Sq. Ft.	760	1,640	300,000	200,000
Library (1)	\$253,764.00	\$530,818.80	\$0.00	\$0.00
Park	\$424,748.80	\$540,183.20	\$0.00	\$0.00
Law Enforcement	\$147,820.00	\$187,780.00	\$71,250.00	\$22,900.00
Fire Department	\$156,978.00	\$199,342.00	\$72,549.00	\$24,310.00
Transportation	\$1,465,454.80	\$2,146,760.00	\$875,799.00	\$418,710.00
General Government	\$109,174.00	\$138,580.00	\$52,650.00	\$16,900.00
Solid Waste	\$147,820.00	\$145,140.00	\$31,200.00	\$9,800.00
School Board (2)	\$1,559,763.20	\$929,240.40	\$0.00	\$0.00
SUBTOTAL	\$4,265,522.80	\$4,817,844.40	\$1,103,448.00	\$492,620.00
TOTAL EST. IMPACT FEE				\$10,679,435.20

Notes:

(1) This fee is collected by the City of North Port for Sarasota County and is based on the average fee.

(2) This fee includes Educational Administrative charge.

(3) Retail impact fees based on Shopping Center > 50,000 sfgla (per 1,000 sq. ft.). Actual fees may be based on specific use.

(4) Office impact fees based on Office, General 100,001-200,000 (per 1,000 sq. ft.)

Source: Impact Fee Ordinance 2014-12, City of North Port, Florida; Impact Fees Charged on Development in Unincorporated Sarasota County (updated 8/18/16)

Figure 3.11.C - Public Utility Capacity Fees

	Single-Family	Single Family (760 Units)	Multi-Family (1)	Multi-Family (1,640 Units)
Water	\$1,735.00	\$1,318,600.00	\$1,445.26	\$2,370,218.20
Sewer	\$2,388.00	\$1,814,880.00	\$1,989.20	\$3,262,294.56
Total	\$4,123.00	\$3,133,480.00	\$3,434.46	\$5,632,512.76

Notes:
(1) Assumes 0.833 ERC (efficiency/one bedroom apartment)

Total Impact and Utility Capacity Fees at Buildout \$19,445,427.96

Figure 3.11.D - Village D Residential Market and Taxable Values

Figure 3.12.D - Village "D" Residential Market and Taxable Values							
Housing Type	Market Value Per DU	Taxable Value per DU (no Homestead)	Taxable Val. Per DU (w/Co. Homestead)	Estimated Ad Valorem Tax Revenue Per Unit	Non-Ad Valorem Assessments (1)	Total Units	Estimated Annual Tax Revenue
Single Family Detached (Type A&B)	\$300,000	\$285,000	\$235,000	\$3,695	\$618	760	\$3,278,129
Multi-Family	\$200,000	\$175,000	\$175,000	\$2,752	\$524	1,640	\$5,372,378
TOTAL	\$500,000	\$460,000	\$410,000	\$6,447	\$1,142	2,400	\$8,650,507

Source: Sarasota County Property Appraiser 2016 Assessments

Notes:
(1) Non-Ad Valorem assessments include solid waste, road and drainage and fire rescue

3.12 Miscellaneous Performance Standards

The following standards are intended to ensure land use compatibility and an attractive community within all of Village D. The following subsections establish additional and specific performance standards for various uses within Village D.

3.12.1 - Village Perimeter Walls

Village Perimeter Walls are permitted within any commonly-owned open space tract or right-of-way within the Village. The village perimeter walls provide identity and definition to different uses and spaces that they separate throughout the village. In addition, these walls provide separation, safety and tranquility for various uses and outdoor spaces in and out of the village. Appropriate locations for such walls are around the Village edges, within the Village Greenbelt, along neighborhood boundaries, along neighborhood center and/or Town Center boundaries, and around any use within a neighborhood or town center. Village Perimeter Walls may be permitted up to ten (10) feet in height, exclusive of any berm. Village Perimeter Walls shall be constructed to resemble one or a combination of the following materials: masonry, wood, PVC, aluminum and wrought iron. Chain link fencing may only be allowed if treated with black or green vinyl cladding and landscaped with a continuous hedge at the base.

3.12.2 - Utility Facilities

Utility facilities such as ground-mounted transformers, wells, storage tanks and lift stations shall be allowed anywhere within the village.

3.12.3 - Temporary Model Homes/Sales Center

Model homes and sales centers shall be permitted anywhere within Village D. Furthermore, a temporary sales center may be allowed along US 41 to facilitate home sales for the village. Model homes and sales centers within Village D may continue to operate until such time as all residences have been initially sold. Model homes/sales centers shall be permitted to include all functions that may be associated with residential sales transactions. Model homes/sales centers may be constructed prior to final certification of all infrastructure in the phase. Up to ten (10) model homes and twenty (20) parking spaces may be constructed in each neighborhood, upon approval of an Infrastructure Plan, identification of a water source, and provision of stabilized access.

3.12.4 - Open Space

Minimum open space criteria shall be established for each area within Village D, as shown on Figure 3.4.A, Main Street Ranchlands Development Standards.

Open space areas include, but are not limited to the following:

- Buffers
- Landscaped areas in off-street parking areas
- Dry detention areas
- Existing or proposed bodies of water, including stormwater management areas
- Active and passive recreation areas, such as playgrounds, golf courses, multimodal trails, and other similar open spaces
- Interior landscaped areas within commercial and mixed use areas
- Building perimeter landscaping
- Pedestrian oriented hardscape areas such as plazas and outdoor dining spaces

3.12.5 – Lighting

In addition to the lighting design standards outlined in Section 10 of the VDPB, Residential Neighborhoods within Village D may, as an alternative to Metal Halide Lighting, use a High-Pressure Sodium Vapor (HPSV) or LED as a softer lighting source. Village D may also utilize the standard poles and fixtures provided by FPL. Sidewalks located along roadways are considered part of the roadway and will comply with the lighting requirements of the adjacent roadway. Other sidewalks will be illuminated per the Pedestrian Pathway Lights standards, per Section 10 of the VDPB, or may be unlit.

3.12.6 – Village D Town Center Canopy Trees

To further the urban design character of the proposed Town Center and the desire to concentrate uses within a walkable, retail and entertainment district, canopy trees may be clustered based upon specific site designs to provide appropriate function and character to the space, as determined by the WVRC. Canopy trees will be utilized to provide natural shading within pedestrian environments and configured to allow sight lines to building facades and signage elements for the purposes of wayfinding. It is critical that trees placed within an urban environment have proper form at the time of planting. Given the variability of the plant nursery market, the Village D VDPP canopy trees will meet or exceed the criteria established by the current edition of the Florida Grades and Standards for Nursery Plants for a minimum of 2-inch caliper Florida #1 to provide greater flexibility in species and insure availability of structurally sound plant materials.

3.12.7 – Tree Canopy Development Standards for Mixed Use Residential Neighborhood and Residential Neighborhood areas

Street trees and required buffer trees shall count for the minimum 35% canopy requirement.

Residential Street Trees - One (1) canopy tree shall be planted within twenty-five (25) feet of the right-of-way of each local street within a residential development for every fifty (50) linear feet, or substantial fraction thereof, of right-of-way when proposed lots have a minimum of one hundred (100) feet of frontage or greater. When proposed lots have less than one hundred (100) feet of frontage, street trees shall be limited to (1) canopy tree per frontage. None of these required trees shall be planted within a public or private utilities easement.

For proposed lots with less than sixty (60) feet of frontage smaller maturing canopy trees or understory trees may be utilized.

The trees shall be spaced no closer together than twenty-five (25) feet, unless a decorative grouping or alternative method is chosen by the developer. Existing native trees should be used to fulfill these requirements wherever they meet the spacing and size requirements and are adequately protected during construction.

Street trees in common areas within the MURN and RN areas, shall be spaced in accordance with Sec. 45-20 of the ULDC.

3.12.8 – Neighborhood Center Public Art

The Town Center landscape and hardscape design, as well as public park design, will meet the requirement for public art through the incorporation of design elements that contribute to the overall theme of Town Center.

3.12.9 – Commercial Floating Docks in Manmade Lakes

Permits shall not be issued for commercial floating docks designed for non-motorized and electric motorized vessels only, including piers, access ramps, terminal platforms, stairways, walkways and mooring pilings, unless and until the following additional specific criteria have been met:

(1) The waterbody accessed shall be greater than ten (10) acres or no less than two-hundred fifty feet (250') wide when measured at the top of bank perpendicular to the location of the proposed floating terminal platform.

(2) The floating terminal platform, together with any catwalks or finger extensions, shall not exceed one

thousand five-hundred (1,500) square feet in area, including areas for storage and/or safe launching, nor one-hundred and fifty (150) feet in total length when measured perpendicular to the top of bank, nor fifty (50) feet in width.

(3) The floating terminal platform shall not exceed two (2) feet in height as measured from the water line.

(4) No railings along the perimeter of the floating terminal platform shall be required. Access to the floating terminal platform by use of a ramp shall have non-detachable handrails affixed on both sides and a pedestrian gate to control access at the top of the ramp.

(5) A permanent sign shall be posted on the terminal platform which reads "NO MOTORIZED FUELED VESSELS ALLOWED ON LAKE".

Village D Town Center/Mixed Use Design Standards

3.13 Village D Town Center/Mixed Use Standards

Introduction

The architectural fabric for West Villages is intended to provide a unique community experience that draws its architectural context from the local identity. The elements in the overall architectural arrangement are designed to encourage individuality. There will be multimodal trails, pedestrian paths and an integrated roadway network connecting neighborhoods to the Town Center. The integrated multimodal network will help to establish a sense of community. With the development of West Villages Town Center, each developer/tenant is encouraged in its design efforts to creatively develop a community atmosphere with canopy overhangs, transoms and signage personalities that will reflect individual product presentation, while retaining the integrity of the overall project.

Although an architectural framework has been established for residential and non-residential development, no prerequisites or predetermined design solutions shall be enforced. Each design shall be evaluated on its individual merit and creativity. It is not the intent to, in any way, constrain the expression of personality. Notwithstanding the above, by virtue of the control of both the quality of materials and range of color scheme choices, a unique community atmosphere which both enhances the individual shops and businesses and establishes a cohesive setting will be created.

Figure 3.13.B



The objectives for West Villages Town Center/Mixed Use development include:

- Achieve harmony of development with neighboring buildings and areas of development.
- Achieve a better environment by employing superior design and planning standards.

Town Center and Mixed Use areas are intended to have a functional and attractive development pattern that promotes a commercial, residential, mixed use, and walkable destination where people can live, work, shop, and play. The following conceptual site plan graphics offer a general example of what may be developed in Town Center. Buildings, landscape, and hardscape areas are intended to provide a comfortable pedestrian and vehicular environment and may include outside gathering spaces such as squares, plazas, and parks.

Figure 3.13.A

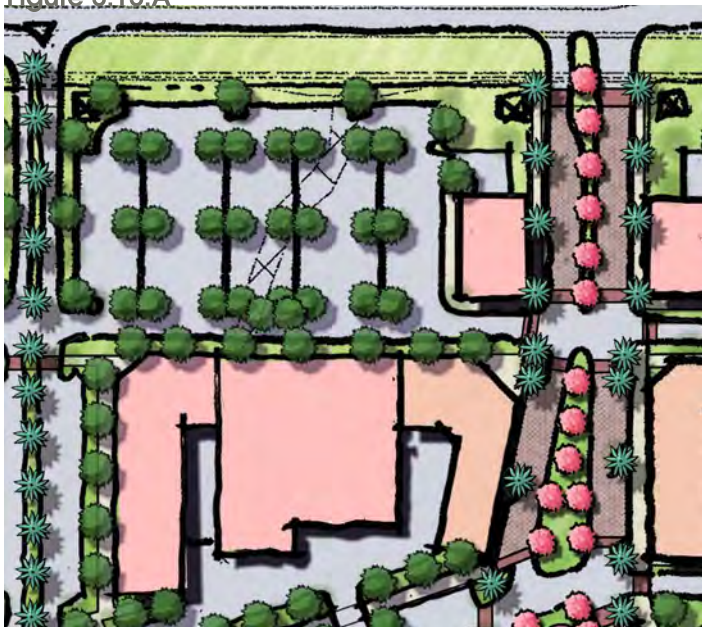


Figure 3.13.C



4. Overnight parking of recreational vehicles and boats, boat trailers or trailers of any kind, and campers or mobile homes may be permitted to park on or near the property, as authorized by the WVRC.

5. None of the vehicles named in 3. and 4. above shall be used as a domicile or residence, either permanent or temporary.

6. Mobile Food Dispensing Vehicle (referred to as “vehicle” in below standards) is any vehicle that is a public food service establishment and that is self-propelled or otherwise movable from place to place and includes self-contained utilities, including, but not limited to, gas, water, electricity, or liquid waste disposal (defined in Chapter 509.102, F.S.).-

(a) Mobile food dispensing vehicles shall be permitted on all ongoing construction sites on a temporary basis during normal hours of construction.

(b) Vehicles shall be permitted within those areas designated as Mixed Use and or Mixed Use Residential Neighborhood, subject to certain limitations, as illustrated on the respective Village Plan. Vehicles shall be identified as a permitted use in the Development Standards table, through the Village District Plan process subject to the following requirements. It is understood such uses may also be permitted within the Mixed-Use Residential Neighborhood providing no alcohol is offered for sale consistent with City of North Port Code Chapter 6, Article I, section 6-4 – Sale near churches or school and 6-5 – Hours of sales.

1) Written permission shall be obtained from the property owner stating that the mobile food dispensing vehicle may operate on the premises and or subject property including any limitations including but not limited to locations, number of vehicles, and placement on the property including setbacks.

2) Except as provided in (b)(1) above, no more than three (3) vehicles may be permitted on a single property at any given time.

3) Vehicles may be located on a lot concurrent with an established principal use or on a vacant lot.

4) Vehicles may be semi-permanent or temporary. Semi-permanent may be located on the property subject to the property owner’s approval. Temporary vehicles are understood to remain in a mobile-ready state and not be permitted to remain on-site overnight. Vehicles shall not count towards the Village’s commercial retail services minimum/maximum land area requirements and not subject to setbacks, height, parking, loading/unloading or similar bulk development standards.

Regardless of if a vehicle is semi-permanent or temporary, all vehicles shall be registered/licensed vehicles.

5) Semi-permanent vehicles shall be connected to public infrastructure including water and wastewater; they shall also be connected to a permanent electrical supply (i.e., “hook up”).

6) No person, natural or corporate, shall conduct any business from within the public rights-of-way except as expressly permitted herein. For the purposes of this section, public rights-of-way are limited to City of North Port public rights-of-way. Specifically prohibited under this section is the sale of food of any kind, goods, wares or merchandise from a vehicle within the public rights-of-way. This prohibition does not apply to private rights-of-way, WVID rights-of-way, private parks and or community spaces identified as part of the VDPP.

7) The vehicle location and surrounding area shall be kept in a clean and orderly appearance. Debris and refuse shall be removed from the property daily. No fluids shall be discharged from the vehicle except for permanent and semi-permanent utility connections.

8) Vehicles shall be located in such a way as to not restrict or interfere with required parking, drive aisles, or ingress and egress. Vehicles shall not create a public nuisance, increase traffic congestion or delay or constitute a hazard to traffic, life or property or an obstruction to adequate access to public safety or sanitation vehicles.

9) Signage, receptacles and similar shall be permitted / provided consistent with Code of the City Sec. 34-55 (e) Placement of items.

10) Confirmation from City of North Port Fire Rescue indicating that the mobile food vendor has passed a fire and safety inspection based on the applicable Florida Fire Prevention Code.

3.13.3 Site Development

Re-platting, subdividing, and establishing required utility easements will be subject to the review and approval, and the requirements of the governing agencies, utility companies, etc.

Design should be conscious of providing an open space network of pedestrian walkways, open areas, and buffers throughout the site. This open space network should be designed to promote and provide pedestrian/bicycle access from the public and private streets to individual buildings.

Connections and transitions to and from adjacent and nearby properties shall be provided through the use of the multimodal

transportation network, as well as appropriately located plazas, public spaces and landscape materials both around the structure and throughout the site.

Areas on site that are not devoted to structures, pedestrian areas, or paved vehicle use areas, must be landscaped and irrigated per the minimum standards in the Landscape and Irrigation Standards Section of the VDPB and/or these Standards. Where conflicts exist, these standards shall govern. A continuous concrete curb shall protect all landscaped areas adjacent to roads and parking areas unless Low Impact Development methods are being used.

No outside antennas, antenna poles, antenna masts, antenna towers, satellite dish, or electronic devices shall be permitted unless screened from view, or as approved by the WVRC.

Temporary construction facilities and signs are permitted during construction. All such temporary facilities and signs shall be further subject to any applicable City codes and ordinances.

Garbage and trash containers or compactors, oil tanks, bottles, gas/LP tanks, tank exchanges and irrigation system pumps may be underground, placed in screened areas, with landscaping. If enclosures are used for screening purposes, walls or fences, up to a height of eight-feet (8'), and finished opaque screening gates shall be used.

All air-conditioning units, mechanical equipment, etc., whether roof mounted or ground mounted shall be contained or shielded to minimize visibility, as determined by the WVRC.

For non-residential uses, provisions may be made through site design, to provide adequate space for service and delivery vehicles to park at the rear of buildings, where physically possible. For buildings that are within 10 feet of an interior roadway that provides on-street parking (i.e. Main Street, Mixed Use Street, see Fig. 2.6.1.B and 2.6.1.C), the available on-street parking may be utilized for service and delivery vehicles where no other alternative exists.

The use of decorative paving is permitted within the project. Suggested finishes and products include colored concrete, concrete pavers, stamped concrete, brick pavers or natural stone suitable for outdoor/pedestrian application.

3.13.4 Non-Residential Signage Standards

3.13.4.1 Sign Criteria

Except for traffic control signage in conformance with the requirements of regulatory agencies and Manual on Uniform

Traffic Control Devices (MUTCD), no signs, either permanent or temporary, shall be erected or displayed on the property, or any building, structure, or window, unless the placement, character, form, size, and time of placement of such sign comply with the standards or comparable standards approved by the WVRC. Said signs must conform to local regulatory ordinances, ULDC/Zoning ordinance, and these criteria. In case of conflicts, these VDPP requirements shall apply.

Building wall signs, logos, and insignia will receive review of the following elements:

1. size,
2. style
3. types
4. placement

National logos, flags, or insignias will be allowed, provided they are sized, executed, placed, and out of the sight triangles.

Building sign design shall be submitted in accordance with the Sign Criteria. Signs shall be either internally illuminated or externally illuminated. No exposed fluorescent exterior illumination shall be allowed.

The WVRC shall review and approve all logos, flags and insignia. They shall determine color palate consistency and compatibility of the architectural finishes for signs, in order to insure overall theme, as well as compatibility between buildings and signage. Sign types and associated permitted square footage of sign face are permitted for each lot, parcel or building and each lot, parcel or building shall be entitled to erect each sign type.

Freestanding/Pylon Signs:

Maximum number of Freestanding/Pylon Signs in Mixed Use Area 1: 1 Sign

Maximum number of Freestanding/Pylon Signs in Mixed Use Area 2: 3 Signs

Maximum number of Freestanding/Pylon Signs in Mixed Use Area 3: 3 Signs

Maximum number of Freestanding/Pylon Signs in Mixed Use Area 4: 3 Signs

Maximum Height of Freestanding/Pylon Signs:
25 feet (US 41); 18 feet (West Villages Parkway)

Sign Area:

Pursuant to ULDC Section 29-12. D.(a). and (b), Freestanding/ Pylon signs shall be permitted a maximum of seventy-five (75) square feet for a building up to one hundred fifty (150) feet in length. For buildings over one hundred fifty (150) linear feet, the sign area shall be 0.50 square feet of sign area per linear foot of building frontage. Maximum sign area for freestanding/ Pylon signs shall be one hundred twenty (120) square feet.

Monument Signs:

Monument signs in Mixed Use Areas: 1 monument sign (maximum height of 10 feet) per outparcel. Monument signs shall be permitted a maximum of 100 square feet per sign face.

Changeable copy/electronic signs shall be permitted as part of a freestanding/monument sign, within Mixed Use Areas.

Gateway Monument Sign:

Gateway Monument signs may include major tenant names for the project.

Maximum Height of Gateway Monument Signs: 25 feet (US 41; 18 feet (West Villages Parkway)

Sign Area:

Gateway Monument signs shall be permitted a maximum of 120 square feet per sign face.

Wall signs:

1) Building Signs

The following parameters apply to building signs. Local authority review, approval and permitting is also required.

All storefront signage and graphics are to be reviewed and approved by the WVRC. All sign packages shall include locations, sizes, colors, style of lettering, materials, types of illumination, installation details, and logo design.

- a) No roof-mounted signs are permitted.
- b) "Box" or "can" type signs are not permitted.
- c) Store identification signs may include trade name. Any crest, shield, logo or other established corporate insignia is permitted and shall be included within the allowable sign area.

d) Any sign, notice, or other graphic display, particularly self-illuminated signs, located in the interior of the premises and easily legible from the exterior, except those which are required by applicable laws, codes, ordinances, or regulations, are permitted, upon approval of the WVRC.

e) All attachment devices, wiring, clips, transformers, lamps, tubes, bulbs, lighting sources, manufacturer's labels or plates, and other mechanisms required for signage shall be concealed from public view.

f) Letters may be interior-illuminated with lamps contained entirely within the depth of the letter.

g) Any structure containing one (1) or more nonresidential occupants shall be allowed to display wall signs as follows:

I. Maximum of four (4) wall signs for that portion of the building that is leased/owned by a tenant/occupant, which is visible and/or accessible to the public. Maximum cumulative sign area shall be calculated as follows: ten percent (10%) of the surface area of the exterior wall included in an occupant's individually leased or owned premises, up to a maximum sign area of one hundred (100) square feet.

a) The following types of storefront signs will be permitted:

- Individual dimensional work or metal back-lit ("halo effect") letter. These letters must be a minimum of 6" in height.
- Internally-illuminated channel letters with opaque metal sides and translucent plastic faces. Letters must be at least 6" high; or
- Exposed neon tubes forming letters and/or logo; may be used in a decorative, as well as informative, manner.

h) No exposed neon cross-over, raceways, ballast boxes, or transformers will be permitted for wall mounted signs. Raceways needed to support otherwise floating letters (not mounted to a wall) shall be painted to match the adjacent building. All penetrations of the building structure required for sign installation shall be sealed in a watertight condition and shall be patched to match adjacent finish.

i) Sign company names or stamps must be concealed.

j) The following types of signs and sign components and devices shall not be permitted unless otherwise approved by the WVRC:

- Boxed or cabinet type, except where totally recessed;
- Cloth, paper, cardboard and similar stickers or decals around or on the exterior surfaces of the storefront;
- Moving, rotating, or revolving signs;
- Exterior changeable letters or signs;
- Noisemaking signs;
- Signs painted directly on the storefront sign band;
- Inflatable signs

2) Project Identity Monument Signs

Project Identity Monument signs shall be limited to the sizes and locations permitted in the ULDC and as approved by the WVRC. Project Identity Monument signs may include major tenant names for the project.

3) Peripheral Parcel Identity Monument Sign

Monument signs shall be permitted. These monument signs may be internally or externally illuminated, as approved by the WVRC.

a. Entrance Features

All entrance features shall correspond with the theming of the development, as approved by the WVRC.

Select colors that are compatible with exterior colors, as approved by the WVRC. Keep color scheme simple, and avoid combinations of primary or day-glow colors.

b. Wayfinding

1) Wayfinding signs shall be a maximum of twenty (20) square feet in copy/graphic area, and a maximum of ten (10) feet in height.

c. Directional Signs

1) Directional signs, symbols or devices relating to traffic, parking, public services, facilities, or warnings on private property include, but are not limited to, “entrance”, “exit”, “slow”, “no trespassing”, “restrooms”, and “telephones”.

2) These signs shall be a maximum of twelve (12) square feet in copy/graphic area, and a maximum of four (4) feet in height.

d. Light Pole Banners

1) Light pole banners may be installed in parking lot areas, along entrances to, and roadways within Mixed Use and Mixed Use Residential Neighborhoods.

2) These banners shall be exempt from regulation of quantity, location, and design.

3) The internal banner designs may be changed without review and approval of the City of North Port.

• Blanks

Sign blanks must be 0.80-gauge aluminum; thicker, 0.125-gauge aluminum should be used for signs prone to vandalism, such as the ‘no motorized vehicle’ signs. Blanks must be covered with reflective sheeting of street transportation quality vinyl. There must be two, pre-drilled, 3/8-inch holes. The holes must be centered horizontally with the center of each hole being 1/2 inch from the top and bottom edges. Corners must be rounded with a 1-inch to 1 1/2 inch radius, depending on the size of the sign.

• Sign Mounting Hardware

8-inch, vandal resistant, steel drive rivets.

• Post Specification

Posts shall be constructed of 1 3/4” x 1 3/4”, 12-gauge, square steel tubing with 7/16 inch, pre-punched knockouts on 1-inch centers. Post lengths must be 6’-0” feet. All steel posts shall be powder coated with color to match the design theme for the development. Prior to fabrication, color samples shall be submitted for approval by the WVRC.

• Sleeve and Anchor Specification

Sleeves and anchors shall be used in locations where it is possible for a vehicle to come into contact with the signpost, such as adjacent to a street. The use of the sleeve and anchor promotes easy breaking away of the sign post in the event of a collision and increases the ease at which the sign can be replaced.

• Installation

Signposts adjacent to streets are to be installed according to the City of North Port Standard Details, and the (MUTCD) with style and finish as outlined in the Pattern Book and VDPP for the proposed development.

In non-roadside locations the signpost can be mounted directly into concrete. The finished height of the post should be 5’-6” feet. Various combinations of signs can be mounted on a single post to address management needs of the particular area.

• Message

Keep the message simple. Use keywords or logos. Avoid redundant wording and unnecessary slogans.

- Lettering

Font must be consistent (or similar styles)

Point size should be relative to the size of the sign.

- Colors

Background must be the same color, consistent with the overall theme of West Villages Main Street, with reflective lettering and symbols. For regulatory signs such as the 'no motorized vehicle' sign, shall be as required by the City .

3.13.5 Non-Residential Building Criteria Building Criteria

1) General

Architectural design of all peripheral property buildings shall be designed to be compatible with one another. Actual color and material shall be approved by the WVRC.

2) Sustainable Construction

All building construction will generally apply elements of the Leadership in Energy and Environmental (LEED) Green Building Rating System or Florida Green Building Coalition (FGBC) Florida Green Development, Commercial, High Rise

Figure 3.13.5.A



Figure 3.13.5.B



Figure 3.13.5.C



Figure 3.13.5.D



Residential and Residential Development Standards, without the requirement to obtain certification from LEED or FGBC.

3) Exterior Design Elements

The style of the development is intended to reflect a variety of architectural forms exemplified by the original architectural styles such as, but not limited to: Neo Classical, Old World European, Florida Coastal, Craftsman, Colonial, Federal, West Indies, and Mission, or other forms, as approved by the WVRC.

Figure 3.13.5.E



Figure 3.13.5.F



Figure 3.13.5.G



Figure 3.13.5.H



4) Exterior Materials

All materials shall be compatible with the overall scope of the community. All materials and colors shall be submitted for review and approval by the WVRC. All materials shall be compatible. Acceptable materials include, but are not limited to:

- Asphalt Shingles (Dimensional)
- Hard Coat Section Stucco
- Clay Roof Tiles
- ArcusStone
- Concrete Roof Tiles
- Finish Concrete
- Cast Stone
- Marble
- Pre-cast Stone
- Brick (possibly painted)
- Metal roof
- Siding
- Stone Veneer
- Natural stone
- Decorative shutters
- Aluminum awnings
- Cementitious siding

5) Exterior Requirements:

The following exterior standards are intended to ensure continuity of style and quality of appearance. This may be accomplished through articulated entrance(s), variegated roof lines, sloping roof planes, ordered variety of window shapes and sizes, and vertically and horizontally varied building masses.

No single architectural style has been established for the surrounding developments.

Materials should be selected which provide an appearance of weight, mass and permanence.

Wood, tile, and metal may be used for accent, embellishment, or accessory detailing only. These materials should generally not exceed 15% coverage on structures. (Windows and door frames, metal roofs and roll up doors are excluded from the coverage limitations).

Entries may be articulated. The use of overhangs, covered entries and courtyard entries is encouraged in order to create interest, provide weather shelter and foster an inviting pedestrian scale.

Figure 3.13.5.I



Color should be integral to the materials; where painted or applied finishes occur, use complementing hues.
Awnings are permitted, but should be used as accent, accessory elements only.

Surface printed signage is permitted as well as cast lighting onto awning surface for graphic illumination and awning accent.

Figure 3.13.5.J



Varied and pitched roof – Roofing finishes, dimensional shape and color shall be submitted for review and approval by the WVRC. Raised seam metal, dimensional architectural asphalt shingle and concrete tiles are encouraged as approved surfaces..

Retaining walls where required, should be carefully integrated into the building form or resolved into landscape materials.

6) Building Height

Maximum building heights are established in the Development Standards of this VDPP (see Figure 2.3.A).

7) Solar Collectors

Solar collectors may be permitted at locations approved by the WVRC.

8) Exterior Planting and Furniture

Open space areas are to be landscaped in accordance with the landscape theme established by the VDPB, as may be amended from time to time, consistent with these Standards, as well as modifications for individual Purchaser/Lessee identity, as approved by the WVRC.

Purchaser/Lessee shall be permitted to place flower pots, window boxes, planters, and furniture within their entrance areas, subject to approval of the WVRC.

All furniture and bicycle racks should be durable and intended for exterior use.

Photograph or cut sheet on all furnishings will be submitted for review and approval by the WVRC, prior to installation or placement.

3.13.6 General Off-Street Parking Lights:

- 1) All general off-street parking lighting fixtures may be either metal halide, LED, or High Pressure Sodium “cut-off” light with a concealed source.
- 2) Light posts shall be round, tapered metal, painted black; or integrally colored, octagonal, tapered concrete to match West Villages standards.
- 3) The color of the light fixture and arm (if applicable) shall be black, unless otherwise approved by the WVRC.
- 4) No general parking lot illumination light shall be attached to any structure.
- 5) The total illumination caused by all property outdoor lighting, including light sources, diffraction, and reflections from on-site objects, shall be limited as shown below.

6) Criteria - Commercial

(min) Average:	5.0 foot-candle for metal halide; 3 foot-candles for LED
(max) Average	10.0 foot candle
No Avg./Min:	
Max/min	10:1 foot-candle for metal halide; 5:1 for LED.
	No Max. foot candle
Min. foot candle	0.5-foot candle (priority design requirement)
Trespass:	0.2-foot candle (max) adjacent to residential 0.5-foot candle (max) adjacent to commercial
Mounting Height:	35’ (Max)
Fixture Wattage:	400 (Max), Cutoff
Source:	Metal Halide (MH), LED or High Pressure Sodium

3.13.7 Tree Canopy Development Standards

Development within Town Center and Mixed Use areas shall be required to provide a minimum of twenty percent (20%) canopy coverage at maturity. This standard is not applied to individual parcels, but to all development areas within West Villages Town Center.

Street trees and required buffer trees shall count for the 20% minimum canopy requirement.

If the development area includes any Heritage trees, those trees may be removed, regardless of size, if replacement trees are either provided to meet inch-for-inch replacement (minimum replacement tree size shall be 2" dbh), in lieu of Heritage tree mitigation fees, or Heritage tree mitigation fees are paid, as determined by the Master Developer. All replacement trees shall count toward compliance with canopy tree coverage requirements.

3.13.8 Landscape and Irrigation Standards

Landscape areas are four (4) separate zones; landscaping against major roads and parkways, against abutting properties, interior landscaping, and building perimeter landscaping.

The WVRC shall have authority to review and approve designs consistent with the Southwest Florida Water Management District (SWFWMD) Design Standards.

3.13.8.1 Landscape Requirements for Major Roads and Parkways

Street trees, shrubs, and sod within the right of way will be provided and installed by the West Villages Improvement District. All landscape requirements between the right-of-way and proposed development will be provided and installed by the developer, unless otherwise specified.

Berms may be used as landscape treatment. No building structures, except walls, freestanding signs, or fencing shall be constructed on the berm.

a) There will be a landscaped frontage yard no less than 15' wide on the Purchaser/Lessee property between the right-of-way line and the paved ground surface area. It will be continuous along the entire right-of-way containing trees, shrubs, ground cover and turf grass (except for pedestrian and vehicular access locations).

b) The frontage yard shall include three (2) canopy trees and three (3) ornamental trees planted every 100 feet, or fraction of frontage yard. No canopy tree shall be planted closer than five (5) feet to the Purchaser/Lessee curb, or closer than seven (7) feet to a sidewalk. See Figure 3.13.A

c) The opaque screen will be entirely of living landscaped material which will be 18" in height at the time of building occupancy and must be continuous along the entire frontage yard. All shrub beds will be mulched.

d) The entire frontage yard shall be irrigated. It is recommended that the main supply line be held tight to the inside Purchaser/Lessee curb (as opposed to the right-of-way line) and to minimize soil disturbance in the natural vegetation areas.

e) When the vehicular entrance/exit intersects a right-of-way or internal drive, all landscaping within the triangular areas described below shall allow unobstructed cross-visibility between 2' to 6'-0" above finished grade. Trees will have all foliage removed (clear trunk) to a height of 6'-0". Only turf or ground cover will be permitted closer than 3'-0" to the entrance/exit paved surface.

The triangular areas are:

- At interior roads, from intersection of the right-of-way line with entrance/exit road edge line to 2 points each 10'-0" along those lines and connecting those 2 points defines the first cross visibility triangle. See Figure 3.13.B.
- At the main entrance drives to large commercial tracts, from intersection of the dedicated right-of-way line of major parkways with an access right-of-way line to 2 points each 30'-0" along those lines and connecting those 2 points defines the second cross visibility triangle. See Figure 3.13.C.

Submitted site visibility triangles will be subject to final approval by the City of North Port's regulatory review process.

3.13.8.2 Abutting Property Requirements

Abutting property requirements will apply between the Purchaser/Lessee and (1) common properties/drives, (2) other adjacent Purchaser/Lessee, and (3) private West Villages space. For all three conditions, there shall be a continuous landscape buffer yard the entire length of the common property between the property line and the Purchaser/Lessee's paved ground surface area. Entrance/exit or cross access paving width plus three-foot each side will be deducted when computing the buffer yard length requirements. The buffer yard minimum width shall be as detailed below. There shall be no ground surface left uncovered (shrubs, ground cover or turf) and it shall be irrigated. All plant materials shall be from Appendix A of the VDPB.

Site improvements shall include coordination and construction of a concrete sidewalk to the adjacent parcel(s) property line to promote positive pedestrian circulation throughout the development parcels (See Adopted Index Maps).

Figure 3.13.8.1.A
Major Roads and Parkways

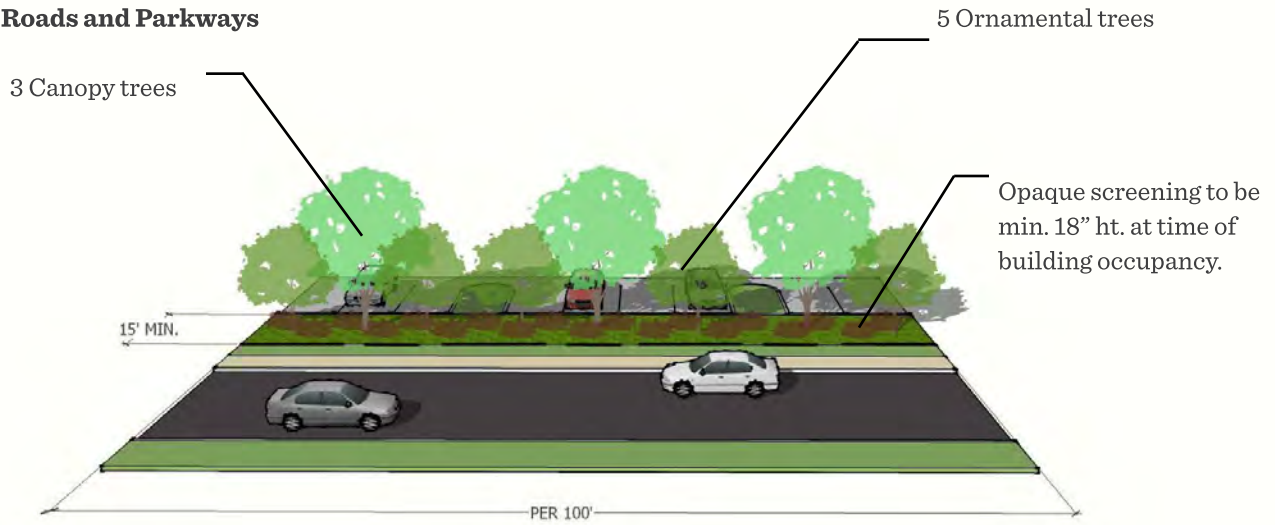


Figure 3.13.8.1.B
Sight triangles

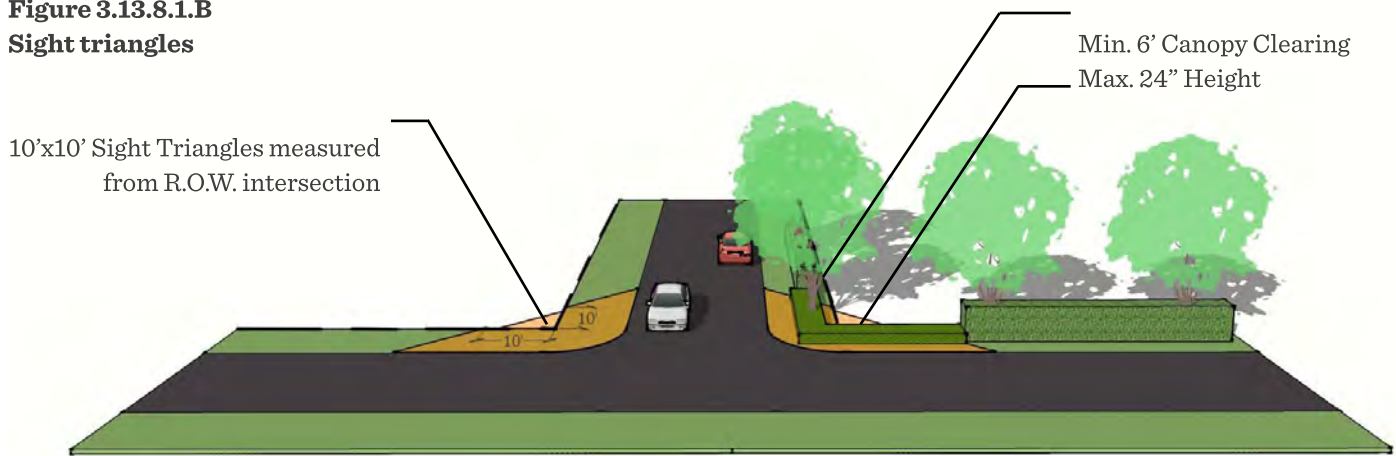
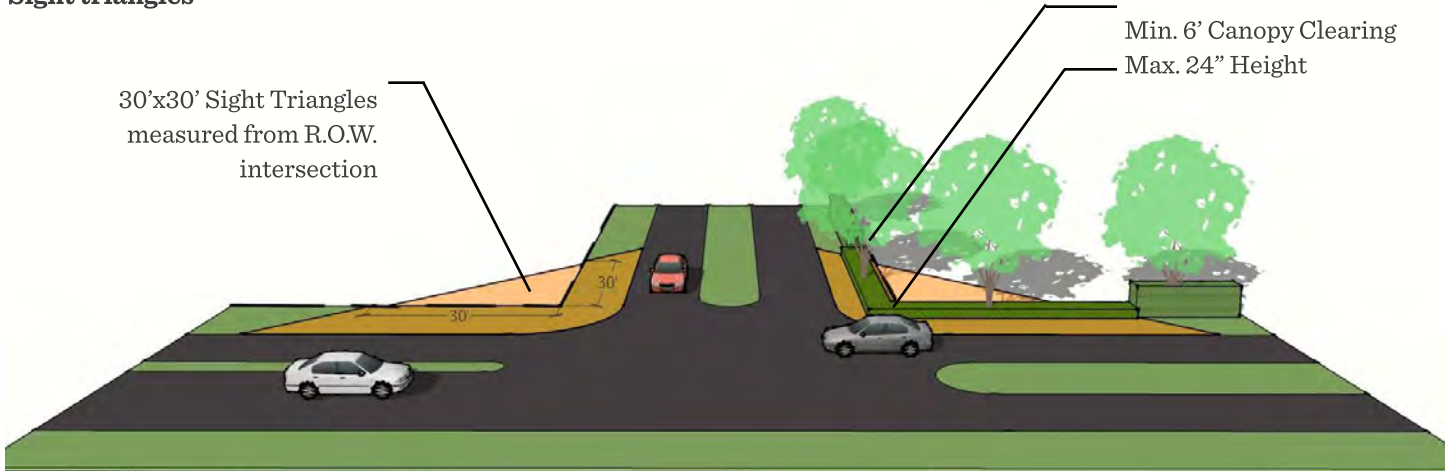


Figure 3.13.8.1.C
Sight triangles



a) Against common internal street/entry drives:

For all buildings that are not built to within 10 feet of internal roadways (US 41 and West Villages Parkway are considered Major Roads and Parkways), there shall be one canopy tree planted for every 50 lineal feet or fraction thereof in a 15' minimum (20' at required sidewalks) width buffer yard. Tree species shall be per Appendix A of the VDPB. For buildings that are built to within 10 feet of internal roadways, no landscaping shall be required, but provided for in the Right-of-Way for those internal streets/entry drives, and consistent with roadway standards in the VDPP. At the discretion of the West Villages Design Review Committee, the Purchaser / Lessee may request up to a fifty percent (50%) reduction in required buffer width, provided enhanced landscaping within the buffer yard is provided to include one (1) canopy tree, two (2) ornamental trees, and ten (10) shrubs planted for every 50 lineal feet or fraction thereof.

b) Against other adjacent Purchaser/Lessee properties:

The buffer yard shall be a minimum of 8' wide. The Purchaser/Lessee who causes initial construction (1st Purchaser/Lessee) shall install canopy trees for every 30 linear feet or fraction thereof, an opaque screen of living landscape material (minimum 24" in height), groundcover and sod.

The second Purchaser/Lessee will not be required to provide or install a landscape buffer yard against the common property line.

Where Purchasers/Lessees abut one another in a common access drive, the required landscape buffer may be deleted.

Internal street/drive buffers shall not be required for the following roadway types: Main Street or Mixed Use Street A (see Fig. 3.7.2.A and 3.7.2.B).

c) Against private West Villages property:

The buffer yard shall be a minimum of 15'-0" wide (Figure 7.E). There will be three (3) canopy trees, five (5) ornamental trees planted for every 100 lineal feet or fraction thereof of the buffer yard as well as shrubs, groundcover and sod. New trees shall be planted to match the species of the West Villages existing trees. Purchaser/Lessee shall plant and irrigate to the right of way line or back of sidewalk within private West Villages property as required.

Where adjacent property is to be developed as a continuation of commercial development and/or parking area, the buffer yard shall not be required. Where future development of adjacent property is residential or unknown, the buffer yard shall be required.

If the adjacent property has provided a buffer which meets or exceeds the buffer width and plant quantities identified, the buffer yard and plantings shall not be required.

3.13.8.3 Interior Landscape Requirements for non-residential and mixed use development

Interior landscape areas are in the parking field between the building perimeter curb and the outer parking lot edge. Interior landscape area requirements are in addition to roadway or abutting property requirements, unless otherwise approved.

Palm trees may be utilized, in lieu of canopy trees in interior landscape areas along the exterior of parking lots.

Sidewalks should provide direct and clear connection from all public rights-of-way to the façade of all development/buildings, as determined by the WVRC. At a minimum, one coordinated pedestrian sidewalk, interior to the development, connecting across and through parking lots to serve buildings and/or sites should be provided. Parking spaces shall not disrupt sidewalk connections to building entries.

A maximum of 20 continuous parking spaces may be provided. Parking islands shall be provided at each end of a row of parking spaces. Each island shall be no less than 8'-0" back of curb to back of curb. Each island shall be planted with one (1) canopy tree, shrubs, and groundcover.

Landscaped divider strips may be provided at an average of every sixth (6th) row of parking, and may be designed with a minimum 10-foot wide strip containing a minimum five-foot sidewalk. Wheel stops shall not be used adjacent to these walkways. Landscaped divider strips, not exceeding a 4:1 slope, may be utilized for stormwater retention purposes.

The WVRC may determine use and location of wheel stops.

Where interior landscape areas abut the roadway, the roadway buffer may serve as the perimeter parking lot buffer. However, where the vehicle use area does not abut a roadway, the perimeter landscaping requirements shall be a minimum width of four (4) feet containing two (2) canopy trees per one hundred (100) linear feet or substantial fraction thereof, and either shrubs, hedges, berms or fences or any combination thereof.

All landscaped buffer areas and sidewalks adjacent to off-street parking areas shall be protected from encroachment of vehicles with curbs and/or wheel stops. Wheel stops and/or curbs shall have a minimum height of six (6) inches above finished grade of the parking area. Wheel stops shall be properly anchored. Where wheel stops are located two (2) feet from the front of a parking space, that two (2) feet is not required to be paved.

Figure 3.13.8.2.A
Abutting Property Against Common Internal Street/Entry Drive

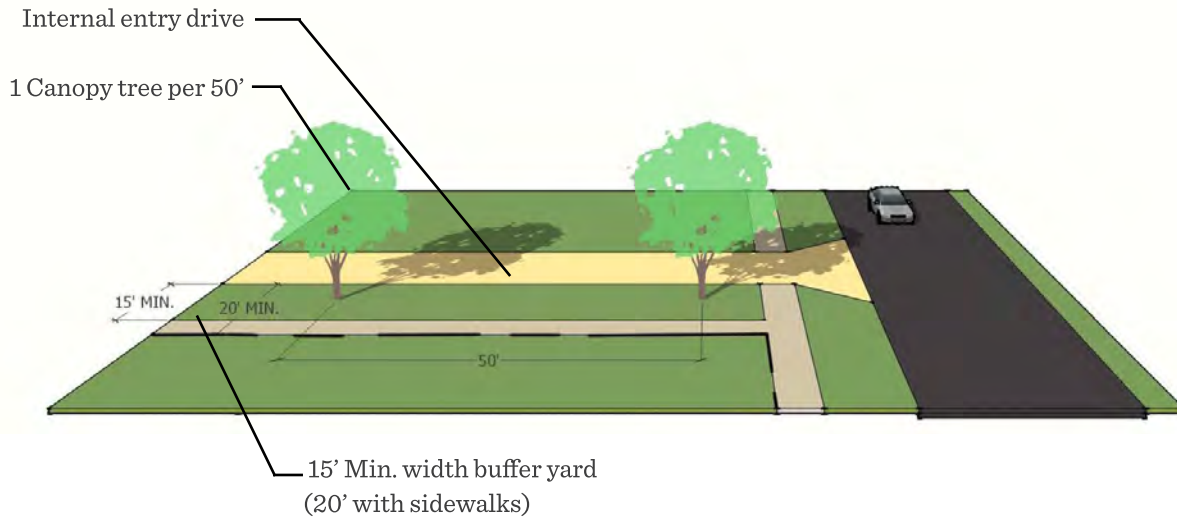


Figure 3.13.8.2.B
Abutting Property Against Adjacent Purchaser/Lessee Properties:

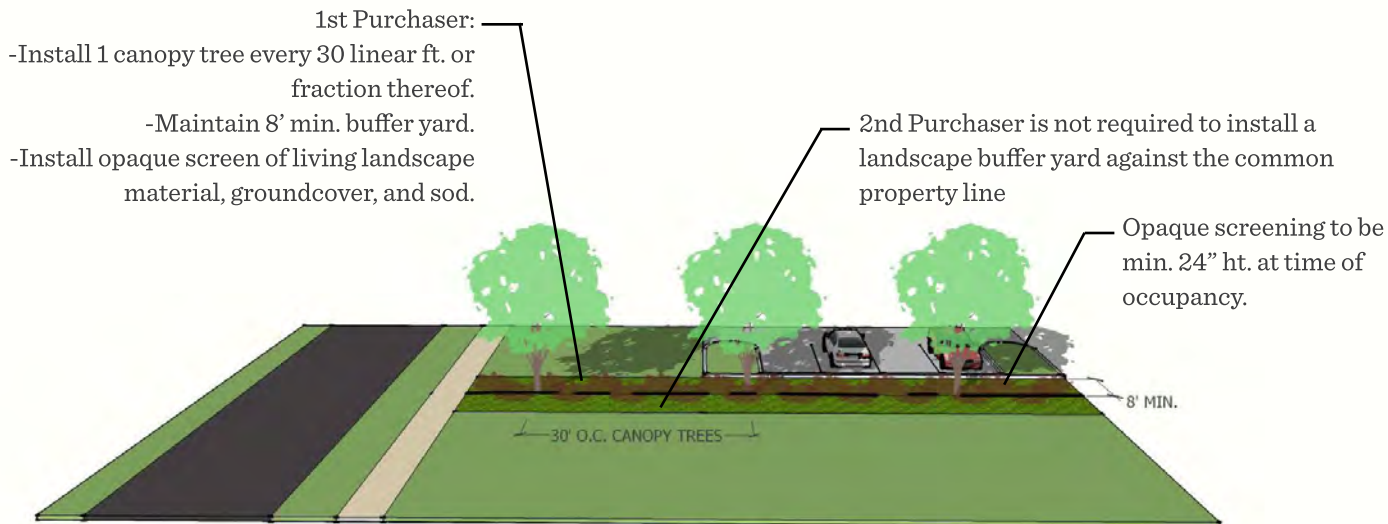
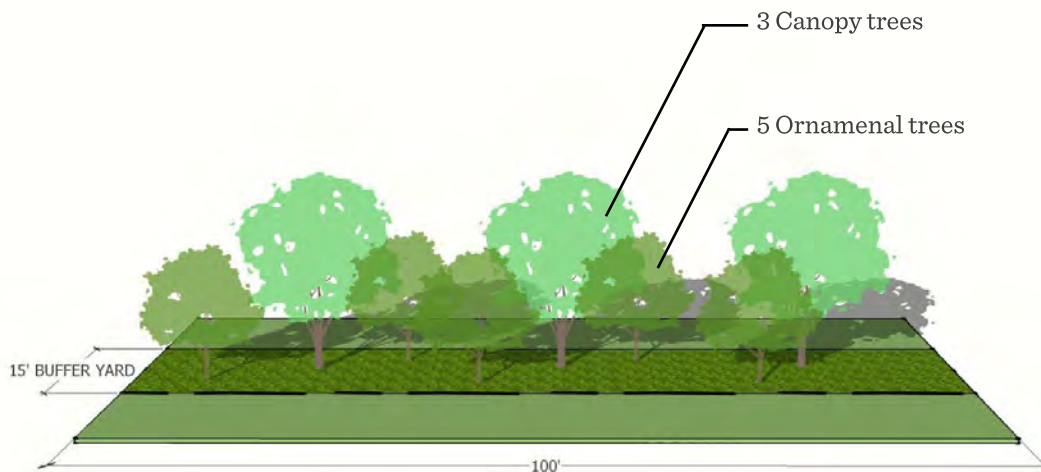


Figure 3.13.8.2.C
Abutting Property Against Private Wet Villages Property



However, the area between the wheel stop and the landscaped area shall receive appropriate landscape treatment, including planting of grass or ground cover. Where wheel stops are not used, all landscape material, excluding sod/ground cover, shall be installed a minimum of two feet from the face of curb. All parking spaces, except parallel parking spaces, that abut landscaped areas or sidewalks within a parking lot shall have wheel stops or curb to prevent obstruction within the landscaped areas.

All canopy trees planted within the parking field, buffers, and building perimeter landscaping shall count toward inch-for-inch replacement tree mitigation.

Prepared planting soil shall provide for drainage and percolation prior to installation of all plant material. Soil shall be free of debris, rubbish, and any plant/root material.

3.13.8.4 Building Perimeter Landscape Requirements

Foundation plantings shall provide visual breaks along monotonous building façades, provide direction to and enhance entrances, and be used or installed to screen mechanical equipment that is attached to or adjacent to the building.

The Purchaser/Lessee is encouraged to employ creative design and include such amenities as earth mounding, water features, landscape lighting, various sidewalk materials and patterns, boulders, sculpture, etc. The design intent should be toward an entranceway that enhances and complements the building's architecture.

Minimum Planting Requirements (Commercial/Mixed Use):

Calculation: One foundation plant or shrub shall be required for every 3 linear feet, and one ornamental tree or palm shall be required for every 30 LF of publicly visible façade (i.e. excluding alley/internal drive or loading area).

Location: Foundation landscaped areas shall be a minimum of 2' in width. Clustered planting locations shall occupy a minimum of 20%, and be located within 30 feet of publicly visible façade.

For buildings that are built to within 10 feet of internal roadways, no landscaping shall be required, but provided for in the Right-of-Way for those internal streets/entry drives, and consistent with roadway standards in the VDPP.

Minimum Planting Requirements (Single-use Residential within Town Center/Mixed Use areas):

Calculation: One foundation plant or shrub shall be required for every 3 linear feet and one ornamental tree or palm shall be required for every 30 LF of all buildings.

Location (Single-use Residential within Town Center/Mixed Use areas):

Foundation landscaped areas shall be located no less than 3' and no more than 5' from the face of the building.

If plantings are clustered, they shall occupy a minimum of 50% of building facade, and be located within 15 feet of the building.

For buildings that are built to within 10 feet of internal roadways, no landscaping shall be required, but provided for in the Right-of-Way for those internal streets/entry drives, and consistent with roadway standards in the VDPP.

The transitional space between public and private space offers an opportunity to establish a "sense of arrival" through the use of specialty paving, plazas, decorative lighting, and site furnishings. The use of such elements shall be encouraged throughout the Village D Town Center and Mixed Use areas.

Screening:

All structures and facilities for trash, storage, loading and outdoor equipment must be screened so as not to be visible from the street and pedestrian circulation areas. At a minimum, screening shall be either an opaque screen of evergreen shrubs and/or evergreen trees or by solid walls at least 6' in height. Shrubs must be at least 4' in height with a minimum 2' spread and trees at least 6' in height with a 4' minimum spread at the time of building occupancy. Shrubs shall be spaced no further apart than 3' and trees spaced at the most appropriate spacing for the species; but no further than 12' on center. Walls should be designed to express the appropriate West Villages theme and be coordinated with the building architecture.

To the extent possible, expanses of solid building wall area without architectural detail should be minimized. Where solid building wall area are without architectural detail, they may not exceed 50' in length without being covered by landscape treatment unless the building wall faces an alley, the rear of another building or a loading/service area that is not facing a street or driveway, and is not visible to the public.

Fifty percent of a blank wall that exceeds 50' linear feet, and is visible to the public, shall either be covered or accentuated with planting areas near the foundation of the building that contain large maturing trees and shrubs/vines. Planting requirements shall be calculated as follows:

For every 10 feet beyond 50 feet of blank wall, one maturing canopy tree or palm cluster, and 3 shrubs or vine shall be provided, and such materials should be planted within 15' of the façade. As an alternative, the required materials may be incorporated into the required foundation landscaping planting areas in those areas where the blank wall exists.

Figure 3.13.8.4.A
Building Perimeter Requirements - Commercial/Mixed Use

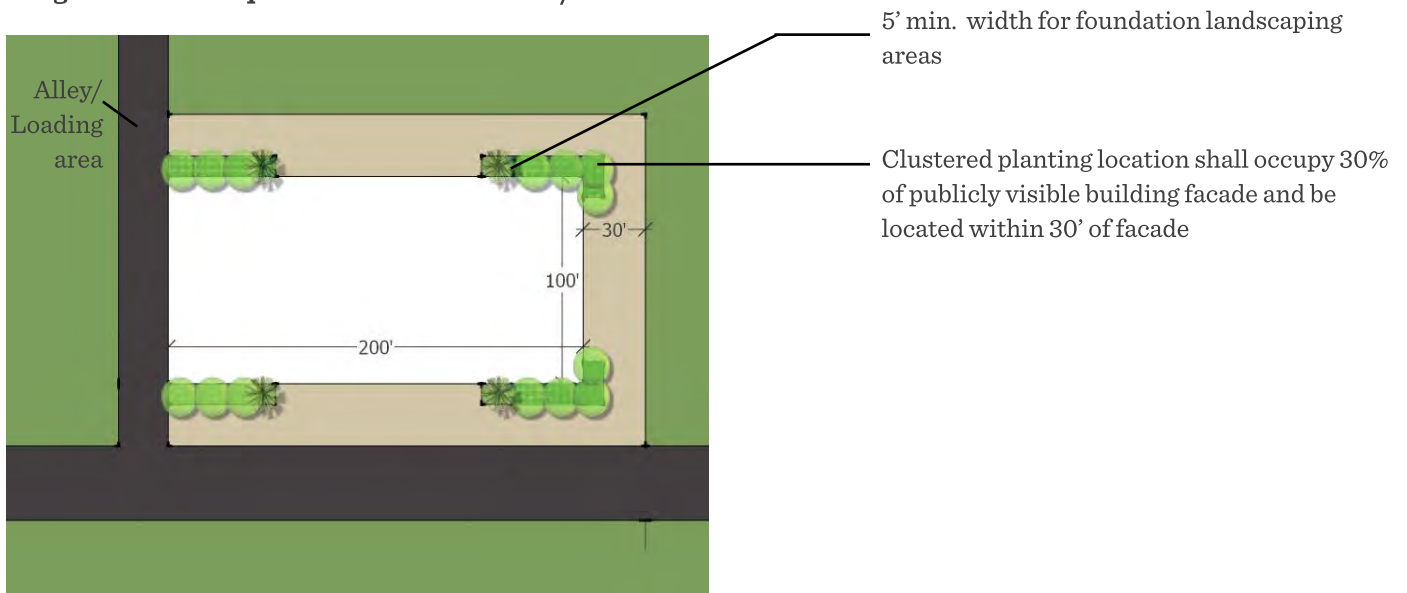
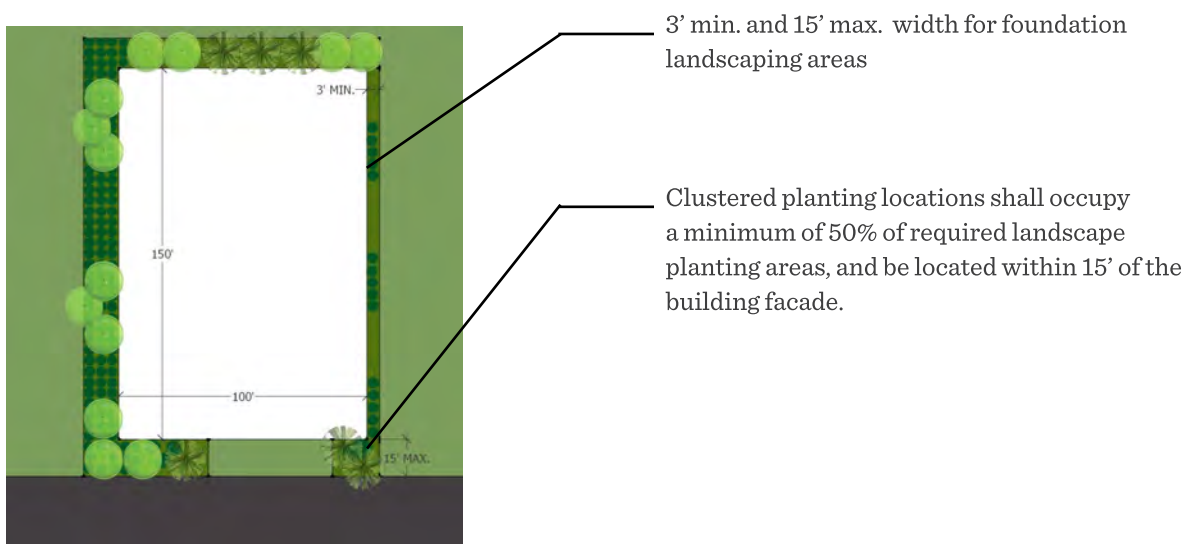


Figure 3.13.8.4.B
Building Perimeter Requirements - Single-use Residential within Town Center



3.13.9 Town Center/Mixed Use Parking Requirements

Required off-street parking facilities shall be located on the same lot or parcel of land that they are intended to serve, except where parking facilities are built to serve the general public and are not intended to serve a single or group of primary businesses or entities.

Each parking space shall have a wheel stop or curbing. Spaces shall be a minimum of 9 feet in width by 18 feet in length. Minimum drive aisle width shall be as follows:

Minimum Parking Standards:

Parking for Mixed Use Buildings may be the sum of the requirements of the various uses computed separately. The total parking requirements for such permitted uses shall be reduced by twenty-five percent (25%) for combined off-street parking facilities, as approved by the WVRC.

The minimum parking requirement for single-use residential buildings in Town Center/Mixed Use areas shall be 2 spaces per unit. Parking for residential buildings may be provided through the provision of structured parking on the lower levels of the building, in an adjacent parking structure, or in parking lots adjacent to the buildings. The parking requirements of this section may be modified through an Administrative Adjustment. If proposed parking is less than the minimum required or more than the maximum allowed, an applicant may submit for an Administrative Adjustment. The submittal for an Administrative Adjustment shall include parking ratios of the same and/or similar uses from the Institute of Traffic Engineers, Urban Land Institute, or other comparable standards used in a municipality. The Administrative Adjustment requires approval by WVRC and the City of North Port's Director responsible for land development services or designee.

Figure 3.13.9.A

Angle of Parking	Aisle Width (Ft.)	
	One-Way	Two-Way
Parallel	12	24
30°	12	24
45°	12	24
60°	18	24
90°	22	24

Minimum Parking Requirements for Non-Residential Uses within Town Center/Mixed Use areas shall be calculated as follows:

Commercial establishments providing drive-up service windows or service lanes shall provide stacking lanes to accommodate 4 cars for each window. Restaurants shall provide stacking lanes to accommodate 5 cars per service lane, measured from the pick-up window. Drive-up service windows or service lanes shall be located to minimize, to the greatest extent, possible, visibility from internal and external roadways.

Figure 3.13.9.B

Commercial/Service Uses	
Office	1 per 500 s.f. of floor area
Services	1 per 500 s.f. of floor area
Finance, Insurance, and Real Estate (FIRE)	1 per 500 s.f. of floor area
Financial Service Center	1 per 500 s.f. of floor area
Other uses not specified in these regulations	Shall be determined by an Administrative Adjustment upon an analysis of similar uses by the City of North Port Director responsible for land development services or designee
Assisted Living Facilities	1 for every 4 beds
Health-Care Facility	1 for each bed, plus 1 for every 3 employees on largest shift
Health-Care Services	1 per 500 s.f. of floor area
Intermediate Care Facility (ICF)	1 for every 4 beds
Health Club	1 per 500 s.f. of floor area
Veterinary Hospital	1 per 500 s.f. of floor area
Artist's Studio	1 per 500 s.f. of floor area
Funeral Home	1 per 500 s.f. of floor area
Research Park	1 per 1,000 s.f. of floor area
Child-Care Center	1 per employee plus adequate and safe provisions for loading and unloading children away from the street and street right-of-way

Retail Uses	
Commercial Use	1 per 500 s.f. of floor area
Retail Services	1 per 500 s.f. of floor area
Big-Box Store	1 per 500 s.f. of floor area
Retail Store, Convenience	1 per 500 s.f. of floor area
Gasoline Station and Convenience Center	1 per 700 sq. ft. plus stacking lanes to accommodate 2 cars for each gas pump island
Retail Store, Express	1 per 500 s.f. of floor area
Hotel/Motel	0.5 per guest room, plus 1 for every 6 employees
Theater	1 for every 6 seats, plus 1 for every 6 employees
Parking/Utilities/Communication Uses	
Utility Box	N/A
Parking Area	As necessary to accommodate use(s) being served
Wireless Telecommunications Towers and Facilities	N/A
Essential Services	N/A

3.13.10 Open access area requirements for nonresidential buildings

All nonresidential buildings that have Fire Department Connections (FDCs) shall provide a twelve (12) foot by twenty (20) foot open access area adjacent to each FDC with ten (10) feet on either side. Such access area shall be accessible by emergency equipment at all times and be centered fifteen (15) feet either side of the connection point.

Fire lanes shall be marked with freestanding signs with the wording, "NO PARKING - FIRE LANE - BY ORDER OF THE FIRE DEPARTMENT". Such signs shall be twelve (12) inches by eighteen (18) inches with a white background and red letters and shall be a maximum of one hundred (100) feet apart.

Definitions	
Residential Uses	
Community Center	Concentration of activities, services, and land uses that serve, and are focal points for, the immediate neighborhoods and used for recreational, social, educational, and cultural activities
Gatehouse	Guardhouse and/or electronic barrier limiting access to a neighborhood.
Single-Family Detached	A building containing one dwelling unit and that is not attached to any other dwelling by any means and is surrounded by open space or yards.
Single-Family Semidetached	A one-family dwelling attached to one other one-family dwelling by a common vertical wall, with each dwelling located on a separate lot.
Townhouse Dwelling	A one-family dwelling in a row of at least three such units in which each unit has its own front and rear access to the outside, no unit is located over another unit, and each unit is separated from any other unit by one or more vertical common fire-resistant walls
Stacked Townhouse Dwelling	Townhouses that have multiple units vertically, typically two in number, each unit having its own private entrance from the street or at least from the outside
Multi-Family Dwelling	A building containing three or more dwelling units, including units that are located over another.
Mixed-Use Development	The development of a neighborhood, tract of land, building, or structure with a variety of complementary and integrated uses, such as, but not limited to, residential, office, manufacturing, retail, public, and recreation, in a compact urban form.
Accessory Apartment	An accessory dwelling unit with a separate means of ingress and egress containing separate kitchen, bathroom, and sleeping facilities, that is (1) physically attached to or contained within an existing single-family house or (2) occupies the second story of an existing garage or accessory building on the same lot as the principal dwelling.
Residential Support Uses	
Park/Recreation Facilities	A tract of land owned or controlled and used by government or specific and designated entities or persons for active and/or passive recreational purposes
Active Recreation	Leisure-time activities, usually of a formal nature and often performed with others, requiring equipment and taking place at prescribed places, sites, or fields
Passive Recreation	Activities that involve relatively inactive or less energetic activities, such as walking, sitting, picnicking, and board and table games.
Religious Use	A structure or place in which worship, ceremonies, rituals, and education pertaining to a particular system of beliefs are held.
Commercial/Service Uses	
Office	A room or group of rooms used for conducting the affairs of a business, profession, service, industry, or government and generally furnished with desks, tables, files, and communication equipment
Services	Establishments primarily engaged in providing assistance, as opposed to products, to individuals, business, industry, government, and other enterprises
Finance, Insurance, and Real Estate (FIRE)	Establishments such as banks and financial institutions, credit agencies, investment companies, brokers of and dealers in securities and commodities, security and commodity exchanges, insurance agents, lessors, lessees, buyers, sellers, agents, and developers of real estate (including drive through facilities)
Financial Service Center	a nonbank entity that does not accept deposits or make loans like traditional banks or financial institution but that provides monetary services that include the sale or redemption of traveler's checks or money orders, money wire transfers, check cashing, and currency exchange
Assisted Living Facility	Residences for the frail elderly that provide rooms, meals, personal care, and supervision of self administered medication. They may provide other services, such as recreational activities, financial services, and transportation.
Health-Care Facility	A facility or institution, whether public or private, principally engaged in providing services for health maintenance and the treatment of mental or physical condition
Health-Care Services	Establishments providing support to medical professionals and their patients, such as medical and dental laboratories, blood banks, oxygen, and miscellaneous types of medical supplies and services
Intermediate Care Facility (ICF)	A facility that provides health-related and personal care services to the chronically ill, disabled, or elderly people, including assistance with dressing and eating
Health Club	An establishment that houses exercise equipment and space for the purposes of physical exercise
Veterinary Hospital	A place where animals are given medical care and the boarding of animals is limited to short-term care incidental to the hospital use.
Artist's Studio	A place of work for an artist, artisan, or craftsperson, including persons engaged in the application, teaching, or performance of fine arts such as, but not limited to, drawing, vocal or instrumental music, painting, sculpture, and writing.
Funeral Home	A building used for the preparation of the deceased for burial and the display of the deceased and rituals connected therewith before burial or cremation
Research Laboratory	A facility for the investigation into the natural, physical, or social sciences, which may include engineering and product development
Research Park	A research facility or group of buildings dedicated to scientific research or business purposes, often linked to a major research facility.
Child-Care Center	An establishment providing for the care, supervision, and protection of children

Retail Uses	
Commercial Use	Activity involving the sale of goods or services carried out for profit
Retail Services	Establishments providing services or entertainment, as opposed to products, to the general public for personal or household use, including eating and drinking places; hotels and motels; finance, real estate, and insurance offices; personal services; theaters; amusement and recreation services; health, educational, and social services; museums; and galleries; self-storage (including drive through facilities)
Big-Box Store	A large, freestanding, single-story retail store, of generally more than fifty thousand square feet, and usually part of a regional or national chain (including drive through facilities)
Retail Store, Convenience	A retail establishment of up to five thousand square feet selling primarily food products, beverages, newspapers and magazines, candy, cigarettes, household items, and a limited amount of freshly prepared foods such as sandwiches and salads for off-premises consumption (including drive through facilities)
Gasoline Station and Convenience Center	A retail facility combining a gasoline station and a convenience store
Retail Store, Express	Smaller version of the large-scale retail store (big-box retailer), serving rural areas or smaller urban areas
Hotel/Motel	A facility offering transient lodging accommodations to the general public and that may include additional facilities and services, such as restaurants, meeting rooms, entertainment, personal services, and recreational facilities.
Theater	A building or part of a building used to show motion pictures or for drama, dance, musical or other stage performances.
Parking/Utility/Communication Uses	
Utility Box	Electric transformers switch boxes, telephone pedestals and telephone boxes, cable television boxes, traffic control boxes, and similar devices.
Parking Area	Any public or private area, under or outside of a building or structure, designed and used for parking motor vehicles, including parking lots, garages, private driveways, and legally designated areas of public streets
Wireless Telecommunications Towers and Facilities	A parcel of land containing a tower, sending and receiving antennas attached to the tower, and a prefabricated or modular structure or cabinets containing electronic equipment
Essential Services	Services and utilities needed for the health, safety, and general welfare of the community, such as underground, surface, or overhead electrical, gas, telephone, steam, water, sewerage, and other utilities and the equipment and appurtenances necessary for such systems to furnish an adequate level of service for the area in which they are located, including community entry way, art, and monumentation



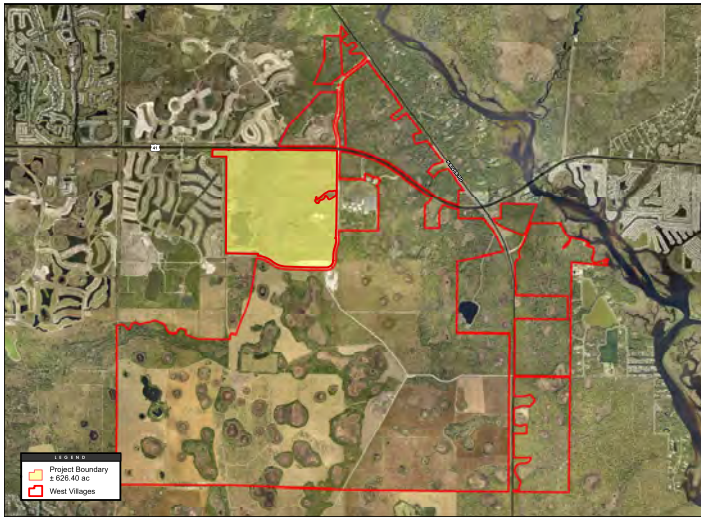
Appendix I - Site Analysis

General Description

1.1 Introduction

The 622.02± acre parcel known as Village “D” is located in the western portion of the City of North Port. The parcel is bounded by US Highway 41 to the north and West Villages Parkway to the east. The surrounding area is transitioning from rural to urban, and is located between Venice’s eastward expansion and North Port’s westward growth. As this development trend continues, the area will continue to transition into a series of urban neighborhoods, with neighborhood and town centers to meet the needs of existing and future residents. This parcel is located within the City’s West Village portion of Thomas Ranch.

This chapter offers a comprehensive site analysis for the subject site, and indicated community need, comprehensive planning, environmental systems, drainage patterns, public services and facilities, community character, and opportunities and constraints to development. The purpose of the analysis is to evaluate these conditions to determine site appropriateness, constraints, and available remedies to facilitate the intended development of the property.



See Figure 1.1.A, Property Site in Exhibits.

Comprehensive Planning and Future Land Use Analysis

1.2 Comprehensive Plan

The City’s Comprehensive Plan has provisions to address growth and development in and around the Village “D” property. The majority of the site is within the “Village” Future Land Use Designation. The balance of the site (126.51+ acres) is undergoing a Large-Scale Comprehensive

Plan Amendment to the “Village” designation. The “Village” designation is outlined in the Comprehensive Plan as a pattern of development that is intended to overcome the problems of urban sprawl; encourage a better job/housing balance; reduce reliance on the automobile while allowing a greater variety of land uses close to home; protect and enhance environmental assets; and provide for an orderly transition from rural to urban. This designation allows for more consolidated development patterns than may otherwise be permitted under typical future land use districts. Additionally, the “Village” designation on the Future Land Use Map is not specific regarding the exact location of individual uses and building locations or types. The Comprehensive Plan outlines a general framework for land development. The surrounding properties to the north, south, east and west, are located in the “Village” Future Land Use designation.

1.3 Future Land Use

Village “D” is located in the portion of the West Villages identified by the City’s Village Index Map as Town Center, which is intended to provide a mix of non-residential and residential development, at higher densities/intensities than the Village designation. The Village Index Map, as adopted in the West Villages Village District Pattern Book, serves as a guide for development of individual Villages within, and defines the limits of the West Villages. This area within these limits is defined as Village on the Future Land Use Map.

The location and size of Parcel “D” are consistent with the Comprehensive Plan, at 626.40+ acres, and located along US Highway 41, at the intersection of West Villages Parkway. Given its location, the subject site is well positioned for a mix of residential, office, retail, civic and light industrial uses, with development of the non-residential uses at a regional scale.

The City of North Port Comprehensive Plan requires development in the “Village” designation provide for an orderly transition from rural to urban land uses. With this in mind, development of the property should ensure appropriate transition from the existing, adjacent residential development to the west and north, as well as a transition from greater density and intensity along US Highway 41 to moderate scale residential density in the southern portions of the parcel. This can be achieved through appropriate location of commercial areas in the northern portions of the site, as well as clustering of units and buffering within a greenbelt along the site’s boundary. The City’s Comprehensive Plan establishes a defined land planning process for any development proposals within the “Village” designation to ensure an orderly and desirable development pattern.

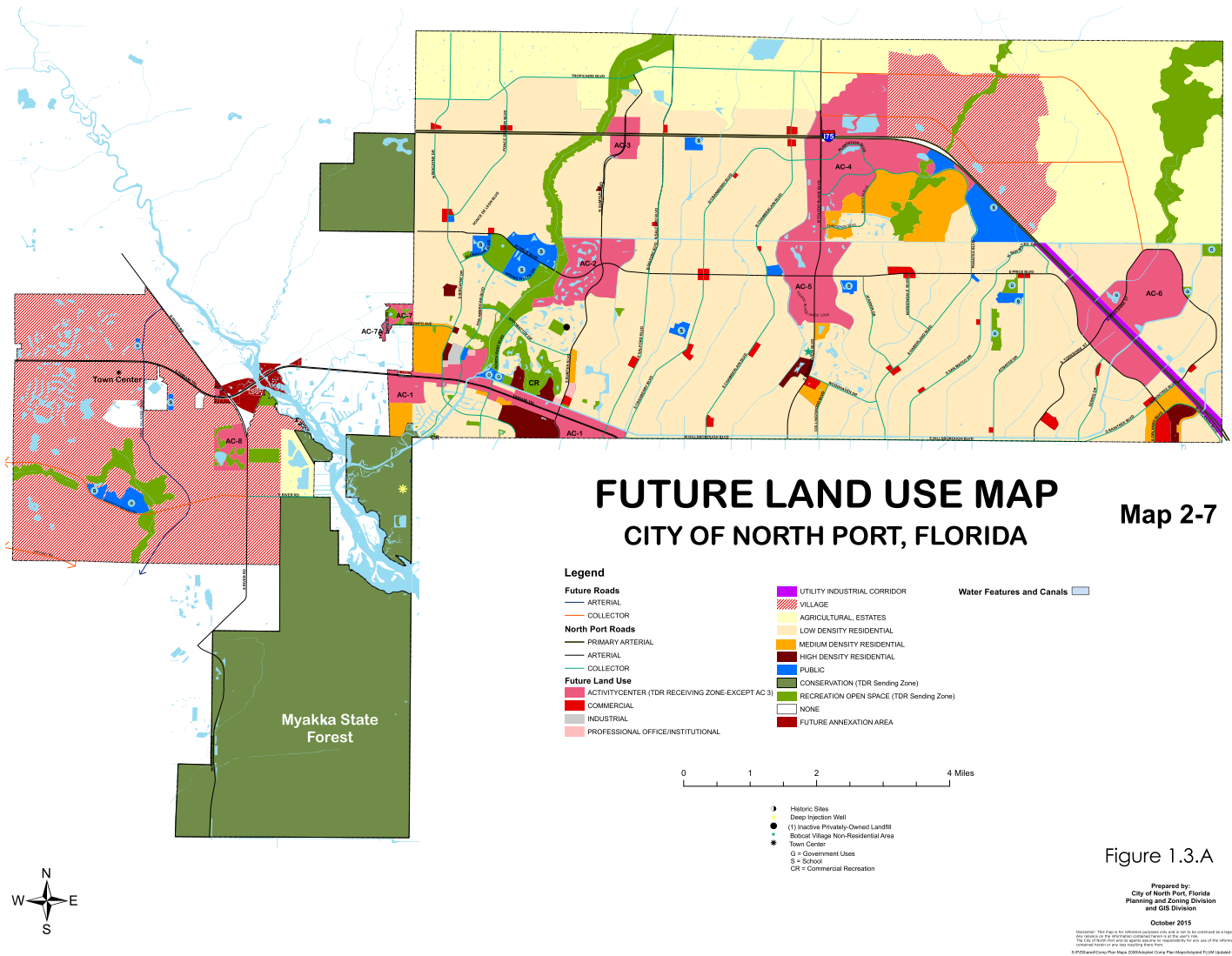


Figure 1.3.A

See Figure 1.3.A North Port Future Land Use Map in Exhibits

Community Character Analysis

1.4 Community Character

The West Village portion of the City of North Port is situated along the US Highway 41 corridor in South Sarasota County east of the City of Venice. There has been sustained residential growth in the area. Lands to the north are located in the Village “A” portion of the West Villages and further beyond that, unincorporated Sarasota County and the City of Venice. The areas to the east are located within unincorporated Sarasota County and beyond that, the City of North Port. The areas to the south are located in the West Villages in the City of North Port and beyond that in unincorporated Sarasota County. Currently, the areas along the US Highway 41 corridor to the east and west of the West Villages represent a suburban character with various residential subdivisions and commercial centers. The

development pattern of the West Villages represents gradual development expansion and infill between the Cities of Venice and North Port.

The urban form along US Highway 41, outside of the West Villages, is indicative of a typical suburban-style development pattern. Individual developments are unrelated and lack cohesion between style and site configuration. The landscape projects an automobile-reliant community character. Retail centers appear disconnected to surrounding residential areas and provide little opportunity for pedestrian arrival. In general, the corridor lacks open space or public civic spaces. The corridor is, however, well maintained and individual developments incorporate enhanced perimeter landscaping.

In general, the suburban-style development patterns are characterized as sprawling developments, predominantly automobile-dependent and arranged in such a way that they are unrelated to adjacent uses. The residential development pattern implemented through North Port by General Development Corporation and in the south Venice Area did not effectively integrate non-residential uses in a sustainable

manner. An opportunity exists to introduce a more effectively integrated approach to development through the inclusion of a balanced mix of uses consistent with the intent of the West Villages.

Village District Planning

1.5 Village District

Village “D” is located in an area identified as the West Villages, which comprises approximately 8,000-acres in the City of North Port. Three villages have either been developed, or are under development and others are in the process of development. The subject site is centrally located along the US Highway 41 corridor. Although each village in the West Villages will be developed by a variety of entities, all villages are required to be planned together, so the result yields an integrated and well-coordinated, sustainable development pattern.

The West Villages Village Index Map and Village District Pattern Book, Figures 1.5.A provide guidelines for development within the approximately 8,000 acres. The plans and pattern book were prepared and adopted to establish a general framework for future development. This plan and pattern book identify the general village boundary, illustrate relationships between villages, designate village and town centers, identify public use sites, designate greenbelts and identify primary transportation corridors. The Index Map and Village District Pattern Book guide land planning for Village “D”.

The Village Index Map serves as a generalized land use map for the West Villages. The Index Map identifies several potential village locations with corresponding letters A through J. The subject site represents a significant portion of the Town Center, as shown on the Index Map.

As noted previously, the 622.02± acre Village “D” is intended to primarily serve as the Town Center for The West Villages. Comprehensive Plan Policy 13.2 and ULDC Section 53-212 specify location and design requirements for the Town Center. According to the Comprehensive Plan and ULDC, the Town Center is to be located in the general area of the intersection of US Highway 41 and West Villages Parkway, a minimum of 300 acres, maximum of 1,000 acres, and is intended to provide a place for residential, office, retail, civic, and light industrial land uses with a regional market base, at a scale that may not be appropriate in the villages.

The location and size of Village “D” are consistent with the provisions of the Comprehensive Plan and ULDC. US Highway 41 is located along the northern boundary. West Villages Parkway has been constructed along the eastern boundary of Village “D”, and is intended to continue south through the West Villages. Village “D” will appropriately serve as a separate, yet well connected Town Center, serving all other villages.

The West Villages Pattern Book establishes standards beyond those established in the City’s land development regulations and Comprehensive Plan. These standards guide development within the public realm, which include street and trail design, Village and Town Center site design, and building design standards for uses other than single-family housing. Development of major streets, trails and individual neighborhoods will follow the standards that are established in the West Villages Pattern Book.

See Figure 1.5.A and 1.5.B, West Village Index Maps “A&B” in Exhibits.

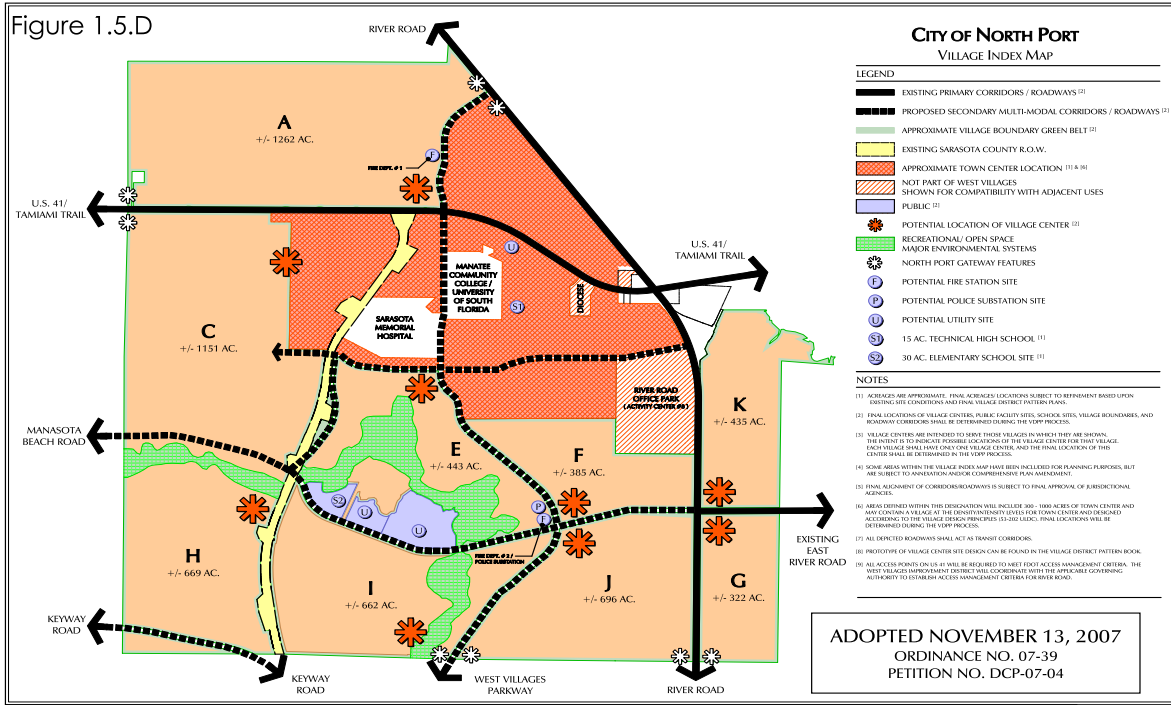
Intent and Need

1.6 Intent

Village “D” is envisioned as a vibrant, mixed use village, and will include retail, office, civic and residential land uses. This village is located adjacent to State College of Florida and a future Sarasota Memorial Hospital medical campus, both located on the east side of West Villages Parkway. Density and intensity of the Town Center is intended to transition from north to south. The most intensive non-residential uses are intended to be located along the US Highway 41 corridor, with a mix of retail/office located adjacent to the more intensive commercial uses. A variety of housing types are proposed for Village “D”, including multi-family, which is intended to be intermixed with the commercial/retail portions of the village. This will create opportunities for a dense, walkable town center, offering housing that will be supportive of the adjacent college campus. The southern portions of the site are envisioned as moderate scale residential neighborhoods. Proposed development will balance the built environment while highlighting and preserving environmental features of the property.

In order to implement the long-range community vision for the City of North Port, non-residential development at the scale that is intended for Parcel “D” is critically

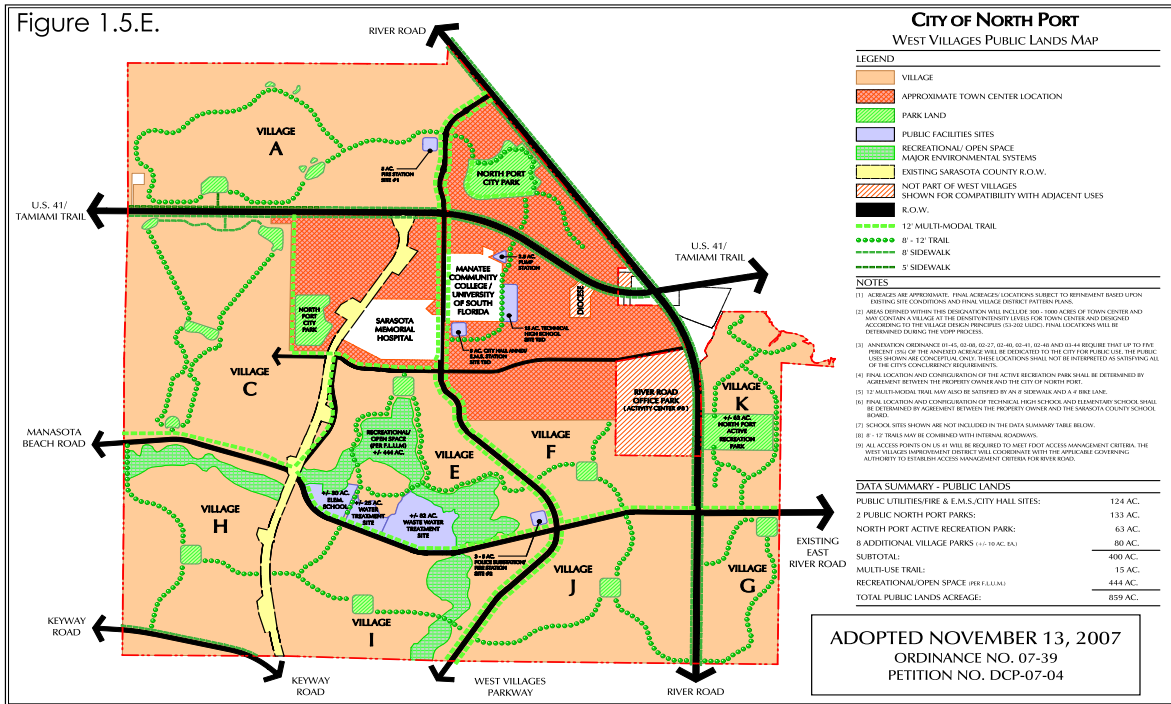
Figure 1.5.A West Villages Index Map "A"



West Villages
West Villages Index Map - "A"

NORTH PORT, FLORIDA

Figure 1.5.B West Villages Index Map "B"



West Villages
4 West Villages Index Map - "B" - Public Lands Map

NORTH PORT, FLORIDA

important. This Town Center will serve existing and future development in the region. Locating such uses in a cohesive manner, providing opportunities for varied modes of transportation to and from the area, and creating a vibrant town center, with formal and informal gathering spaces, furthers the sense of community and sense of place that can be offered through thoughtful and purposeful master planning efforts. The result will be a vibrant town center and nearby neighborhoods, a welcome addition to the City.

1.7 Need

One of the main tenets of the West Villages is the creation of districts within the City that create an identifiable “sense of place” where an area can be considered unique to its setting. This vision is intended to encourage a balance of housing, ensure economic sustainability, reduce reliance on automobile travel, protect the natural environment, and provide for an orderly transition between the urban and rural landscapes. There is a particular need to create places where citizens can interact in a pedestrian-scaled environment that is balanced and complementary to the existing, natural environment. Finally, there is a need to provide a variety of land uses in close proximity to reduce travel distances between residences and other community uses.

The City of North Port has established the “Village” Future Land Use Designation to provide harmonious development patterns that are intended to be more sustainable than the historic suburban style development pattern that can be found elsewhere in the City. In general, the City’s Comprehensive Plan identifies the need for a more traditional, yet comprehensive approach to land planning and urban form, as it relates to the subject site and its neighboring properties. The provisions of the “Village” designation will be discussed in greater detail in subsequent sections of this report.

Village “D” is intended to implement the established need and vision for the City of North Port. The broad range of uses, at varying densities and intensities, represents sound land planning practice and sustainable community design that will serve to create a strong sense of place in the West Villages.

Drainage Analysis

1.8 Drainage

The existing drainage features on the proposed site provide a natural drainage pattern that the proposed purpose

and site development will be able to maintain. Incorporating these natural drainage features into the site design will maximize the functionality and beauty of the Village “D”. These features include the site’s soil, topography, plant material, and existing drainage system.

The predominant soil types within the project boundary per the USDA Soil Conservation Service (SCS) are Soil Number 10 (EauGallie and Myakka Fine Sand) and Soil 31 (Pineda Fine Sand). These soils have an average seasonal high water level (SHWL) of 0.5 feet to 1.5 feet and 0 feet to 1.0 foot below existing grade, respectively. This information comes from Table 14 in the SCS Soil Survey of Sarasota County. From topographic information, the northern half of the site is generally at elevation 8.0 - 9.0 National Geodetic Vertical Datum (NGVD); the southern half of the site is generally at elevation 9.0 – 10.0 NGVD. These elevations are consistent with the drainage pattern which is from the south to the north, ultimately discharging the site through ditches that are culverted under US Highway 41 to reach the Myakka River.

This project area contains 16 wetlands and one large existing borrow pit. Additionally, there are three primary drainage ditches that transverse the northern half of the site. These interconnected wetland and other surface water systems provide the site drainage previously mentioned that ultimately flows under US Highway 41. These culverts are within the Florida Department of Transportation (FDOT) right-of-way.

The Federal Emergency Management Agency’s (FEMA) Flood Insurance Rate Map (FIRM) from 1984 and FEMA’s preliminary draft FIRM from 2014 were evaluated for the existing floodplain conditions. The FIRM from 1984, Panel Number 125144 0375 D, indicates the entire project area is within Zone B. Zone B per FEMA definition is outside of the key floodplain hazard area. The preliminary draft FIRM, Map Number 12115C0365F, also shows the entire project area outside of the hazard zone. This draft FIRM categorizes the site as Zone X. As a result of these indications by FEMA, floodplain compensation is not required for this site.

Due to the location of the site in relation to US Highway 41, which is FDOT right-of-way, coordination will be required with FDOT to ensure the continual conveyance of the site runoff through their conveyance system. If the project has any post-development discharge into the FDOT right-of-way, an FDOT drainage connection permit will be required. As previously stated, the site generally drains from south to north, toward US Highway 41.

Public Facilities Analysis

The site is currently served by a variety of public services and facilities, including roadways, schools, fire, police, and transit, which include northbound and southbound bus stops adjacent to the site on US Highway 41 at the entrance to State College of Florida. Water and sewer infrastructure is also available to the property. Formal agreements have been executed to ensure availability concurrent with site construction.

1.13 Water and Sewer

Water, sewer and reuse infrastructure is available to the property. Formal negotiations are underway to ensure availability concurrent with existing through mains in adjacent roadways. The agreement to accommodate long- and short-term service was formally initiated through a memorandum dated September 18, 2000 regarding water and sewer availability for the West Villages. The City's memorandum includes the anticipated responsibilities of both the City and the West Village Improvement District for providing utility service during interim and final build out periods for the area. At present, the City has water and sewer infrastructure capacity to serve the subject site; however, additional utility infrastructure is anticipated, and includes water and wastewater treatment plants, intended to provide service to this and other Villages. The West Villages Improvement District (WVID) was formed to provide a mechanism to construct the utility infrastructure required to serve new development within the West Villages.

At this time, water and sewer infrastructure sufficient to serve portions of Village "D" will be provided through the off-site 16" water main and 12" force main that connect to existing City plants. A potable water pump station and storage tank has been constructed to ensure adequate pressure during peak domestic and fire demand.

It is anticipated that water for irrigation will be supplied by the WVID through an agreement with EWD and may be augmented through withdrawals from wells and surface waters as permitted by existing water use permits, in this interim period. The long-term service requirements of the West Villages are planned to be served with centralized water and sewer systems, as well as a dispersed reclaimed water system. It is anticipated that new water and wastewater treatment plants will be co-located and constructed within the West Villages. The WVID will be required to design and construct water and wastewater treatment plants, then ultimately turn them over to the City to own and operate.

1.14 Roadways

The site is served directly by US Highway 41 along its northern boundary and West Villages Parkway along its eastern boundary. US Highway 41 exists as a four-lane divided arterial roadway and serves as the primary east-west connection between the Cities of North Port and Venice, as well as a regional arterial along the west coast of Florida. This roadway has a Florida Department of Transportation (FDOT) adopted level of service (LOS) of D, while the City of North Port has an adopted LOS of C on all other roadways. According to the most recent data from Sarasota County, the segments of US Highway 41 to the east of River Road are presently operating at a LOS of B and the segments to the west are operating at a LOS of A.

River Road is anticipated as an indirect route for residents and visitors to Village "D", providing a direct connection to Interstate 75 and Englewood. River Road has an adopted LOS of C, and is operating at a LOS of B.

West Villages Parkway also serves the site, providing a connection from US Highway 41. West Villages Parkway will eventually serve as a major north-south corridor through the West Villages. This road has an adopted LOS of C and is operating at a LOS of A.

1.15 Schools

Development of the subject property may impact the following schools: Taylor Ranch Elementary, Heron Creek Middle School, and North Port High School. These schools are nearing capacity; however improvement plans are being implemented to address residential growth in and around North Port. There are plans to construct two additional classroom wings at North Port High School, and Cranberry Elementary is a new school that recently opened.

1.16 Fire Rescue

Fire service is currently provided by Sarasota County Fire Department Station (#26), located just east of the site, adjacent to State College of Florida. The County's facility currently serves the area, through an inter-local agreement between the City and County. As development continues, a City-owned and operated facility will eventually be constructed to serve the West Villages.

1.17 Police Protection

The City of North Port Police Department provides police protection to the West Villages. Officers patrol various sections of the City. In the event of an emergency, officers

are dispatched from their patrolling positions. The Police Department has not indicated any challenges to police service in the area.

1.18 Transit Service

The subject site is currently served by public transit. Sarasota County Area Transit (SCAT) has two bus routes that travel along US Highway 41, providing linkages between the Cities of North Port and Venice. These routes include #9 and #90X.

Opportunities and Constraints

1.19 Opportunities

Several features of this site can be considered opportunities for Town Center development. In that same vein, there are features of the site that may be considered constraints. There are opportunities for preserving and highlighting certain environmental features as site amenities or assets, both within neighborhoods, and in the Town Center.

In particular, the site's existing land uses and environmental features will inform the future form of development on the site. Other features may inform design options for the site, which may include density and intensity, roadway alignments, overall stormwater design and urban form. There may be limits to the extent to which certain features may be altered, due to systems that go beyond the boundaries of this parcel, or due to the larger vision for the West Villages, as a whole. Certain opportunities and constraints will guide development, density and size on this property. These features will be further explored in this section.

Village "D" is one parcel within the larger West Villages. Consistent with the long-range vision for the West Villages, preliminary development guidelines have been developed to ensure development in the region is logical, rational and harmonious. The Village Index Map is the guiding land use plan for the region. The West Villages Village District Pattern Book is the document that guides general design for site development, roadways, and minimum architectural standards for buildings. Development is required to be consistent with the Village Index Map and Pattern Book for the West Villages.

Village "D" is located in the Town Center, which is intended to provide a mix of residential and non-residential uses to serve the region. The ranges of uses that may be considered include retail, office, mixes of residential, and light

industrial. The vision for Village "D" is to serve as the Main Street for the West Villages, offering a variety of commercial and residential land uses that will support the entire region.

The Index Map identifies the West Villages Parkway along the eastern boundary of Village "D" to be a four-lane divided thoroughfare that will serve as an alternate connection between River Road and US Highway 41. A second corridor is envisioned for Village "D", to serve as a north/south collector roadway through the West Villages, serving to distribute traffic and offer alternatives to future residents. In order to bring commercial development to the West Villages, signalization along US Highway 41 will be a critical component to future development of the Town Center. Enhancements to the roadway network will serve as significant opportunities for future development within Village "D".

1.20 Constraints

One of the primary goals of the Village District is to reduce reliance on the automobile, promote walkability and encourage alternative and sustainable modes of transportation. US Highway 41 is an important regional facility and brings with it development opportunity and visibility for the non-residential land uses that are envisioned within the Town Center. US Highway 41 also brings development constraints, because it is a state road, with access management criteria, that specify access points. Walkability and alternative modes of transportation to and from Village "D" from Gran Paradiso and Village "B" will be somewhat limited, because of US Highway 41. Access management criteria will inform site design for Village "D". With existing and future north/south roads within Village "D", state access management standards will be met.

Historical use of the site and environmental features will inform the location and extent of development on the site. Preliminary environmental investigation shows that the site contains a number of wetland communities. Impacts will be limited, due to state and local regulatory requirements. These features can be incorporated into the site design to serve as community assets and important features of the site. These wetland systems will inform village design.

Existing surface water features of the site include ditches, drainage swales, a large borrow pit and stormwater ponds. These features are consistent with the historical agricultural and ranching operations of this land. Some of these features may be adjusted or changed, to the extent allowed, through the development process. The opportunity exists to highlight the borrow pit as a lake that could become a neighborhood amenity that could serve to offer highly

desirable, premier home sites within the southwestern portion of the village.

All opportunities and constraints that have been described in this section will shape the future village on this property. Some features will limit development options, whereas others may serve as critical design elements that will shape a vibrant, attractive and sustainable village. These conditions were identified after conducting a comprehensive site analysis. These findings will be the framework for the next steps in the Village District planning process.



Exhibits

Figure 1.1.A Property Site

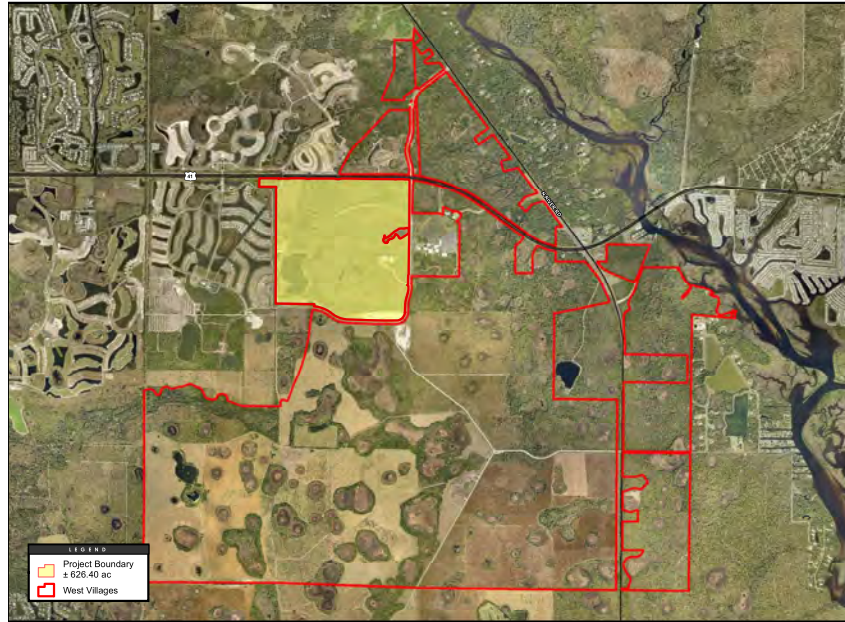
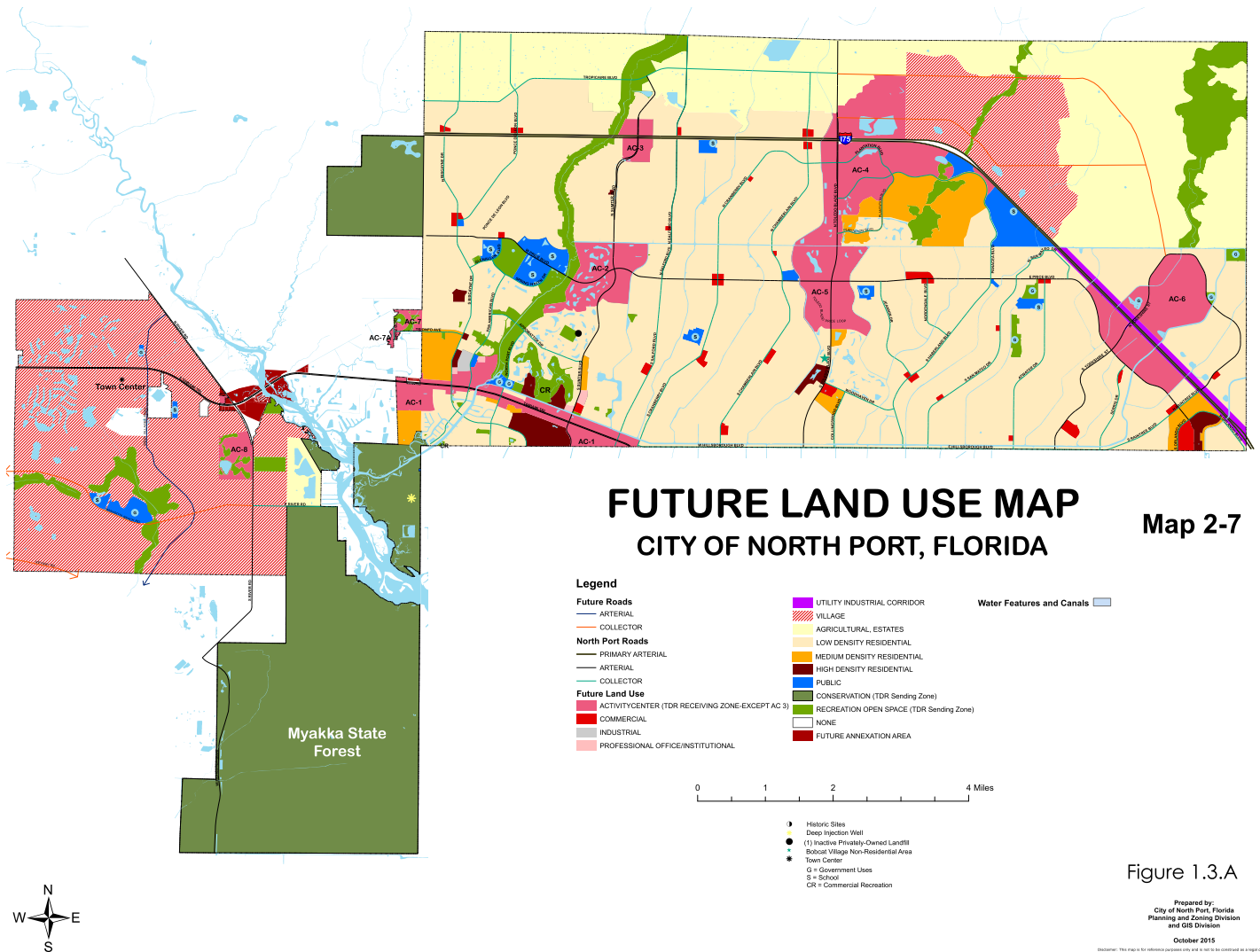
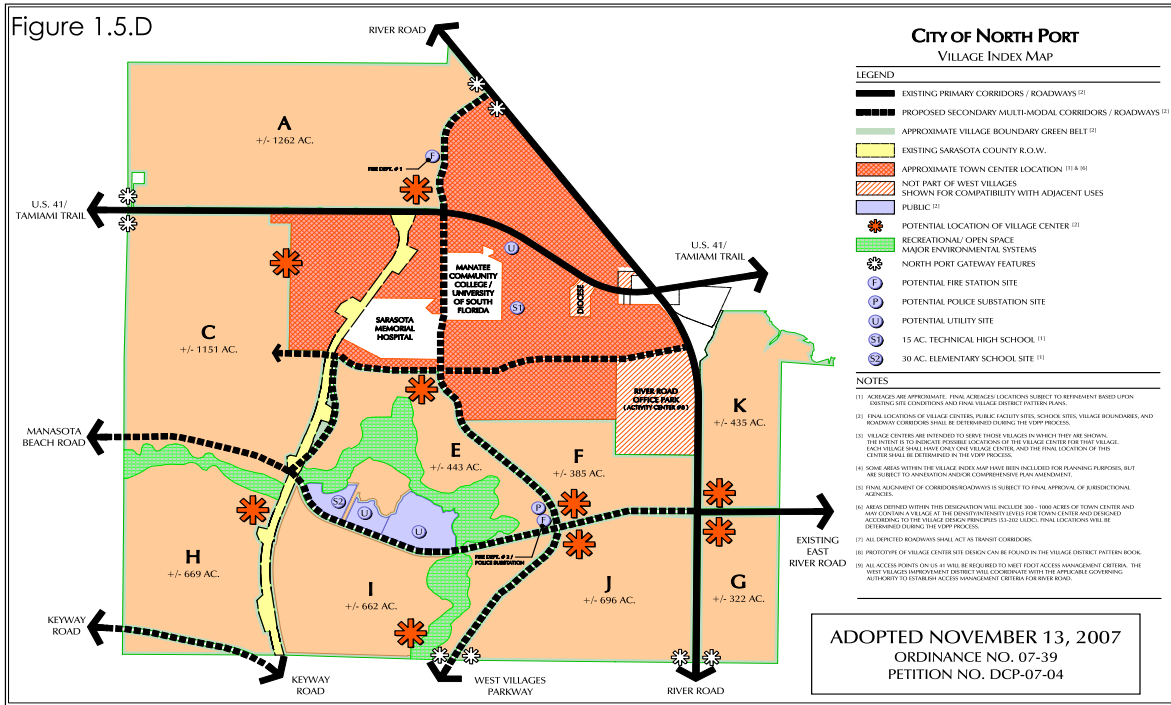


Figure 1.3.A North Port Future Land Use Map



October 2015
 Prepared by:
 City of North Port, Florida
 Planning and Zoning Division
 and GIS Division

Figure 1.5.A West Villages Index Map "A"



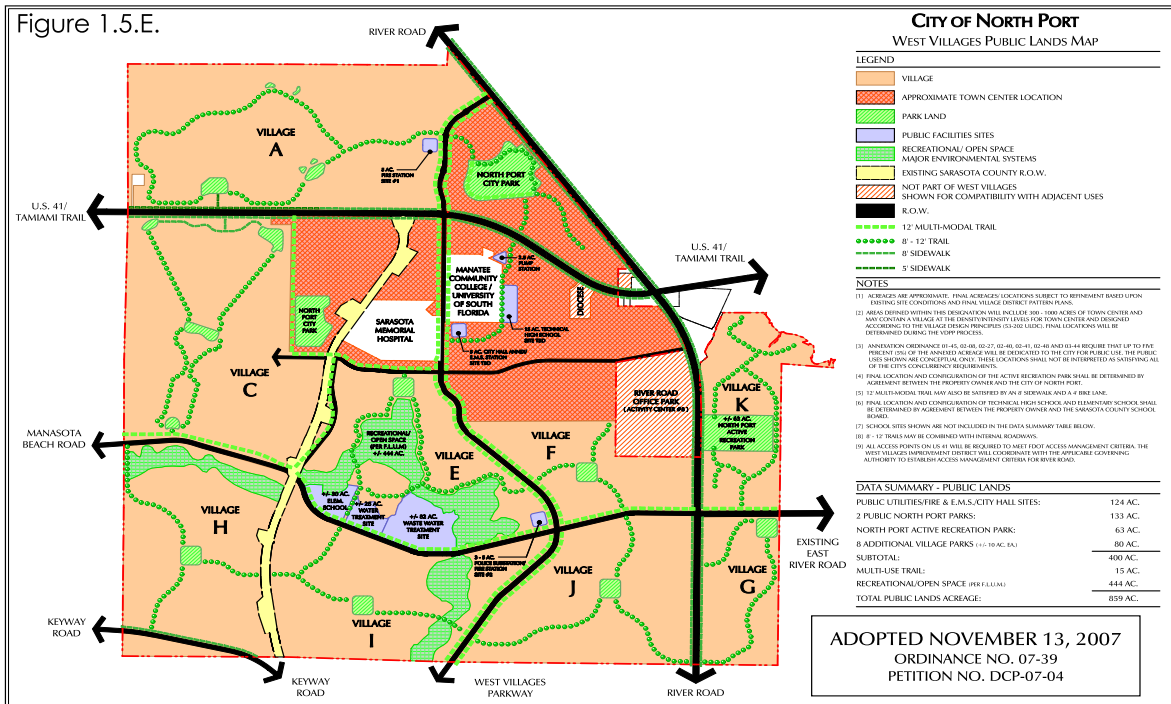
West Villages

West Villages Index Map - "A"

NORTH PORT, FLORIDA



Figure 1.5.B West Villages Index Map "B"



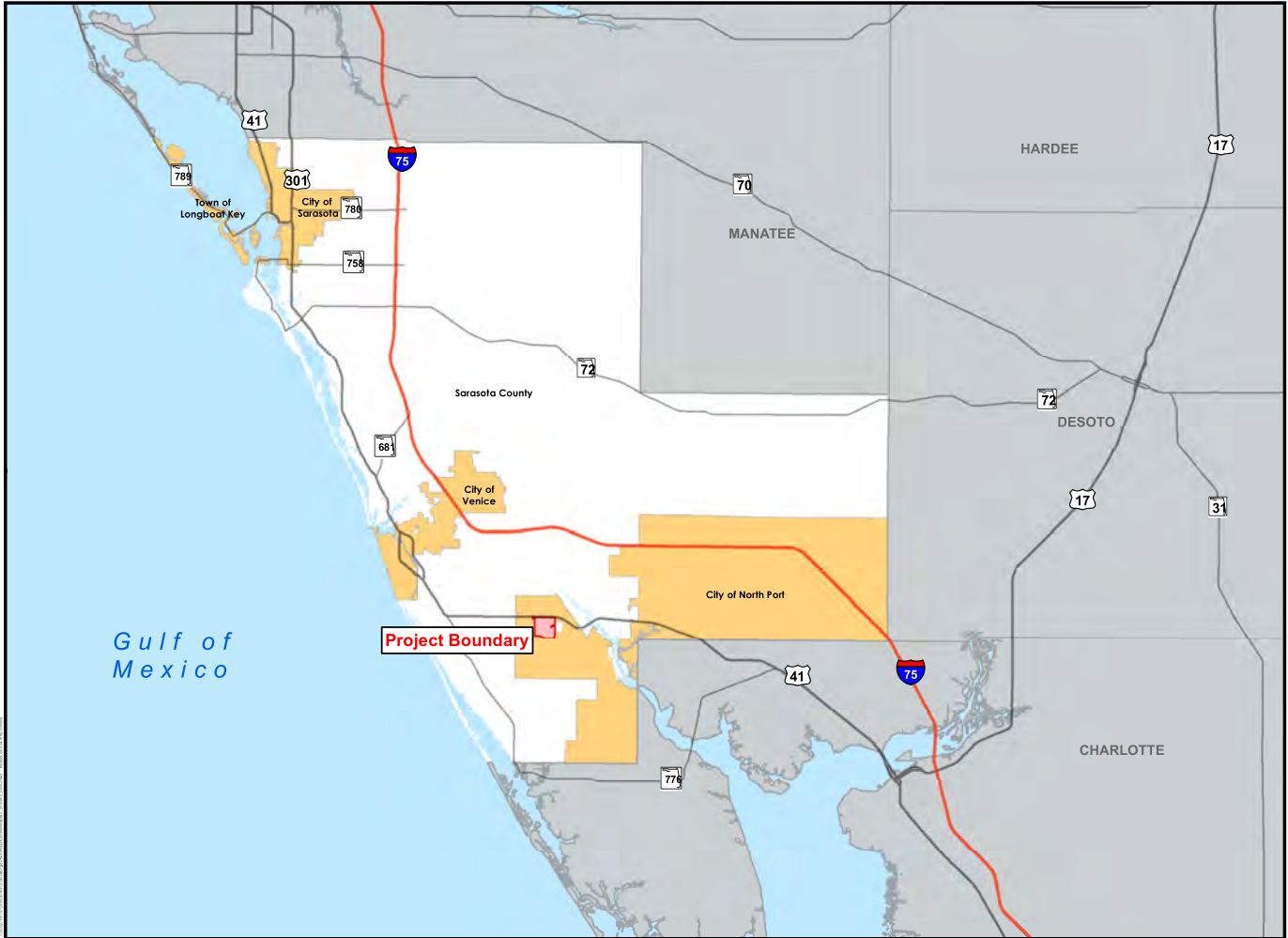
West Villages

4 West Villages Index Map - "B" - Public Lands Map

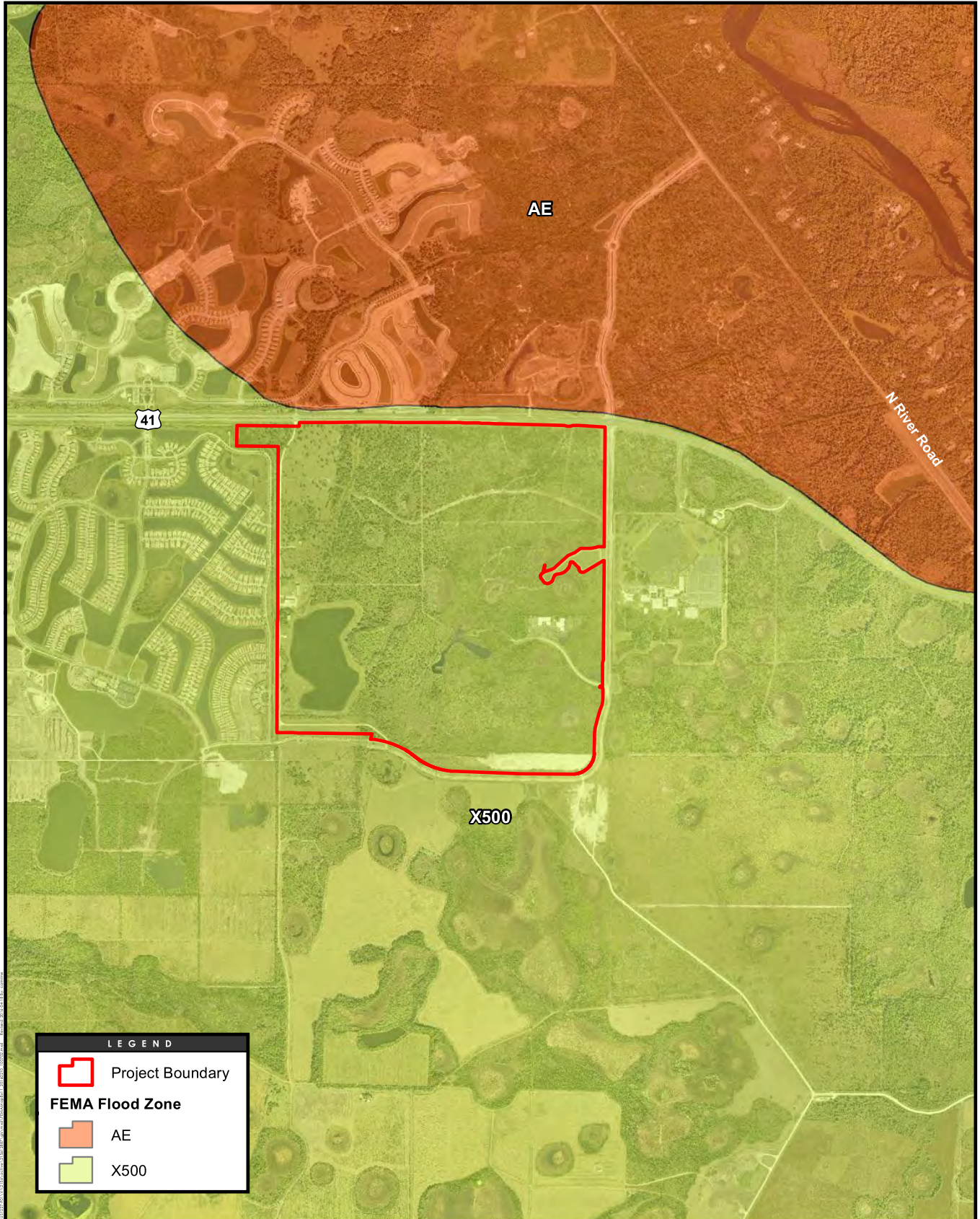
NORTH PORT, FLORIDA



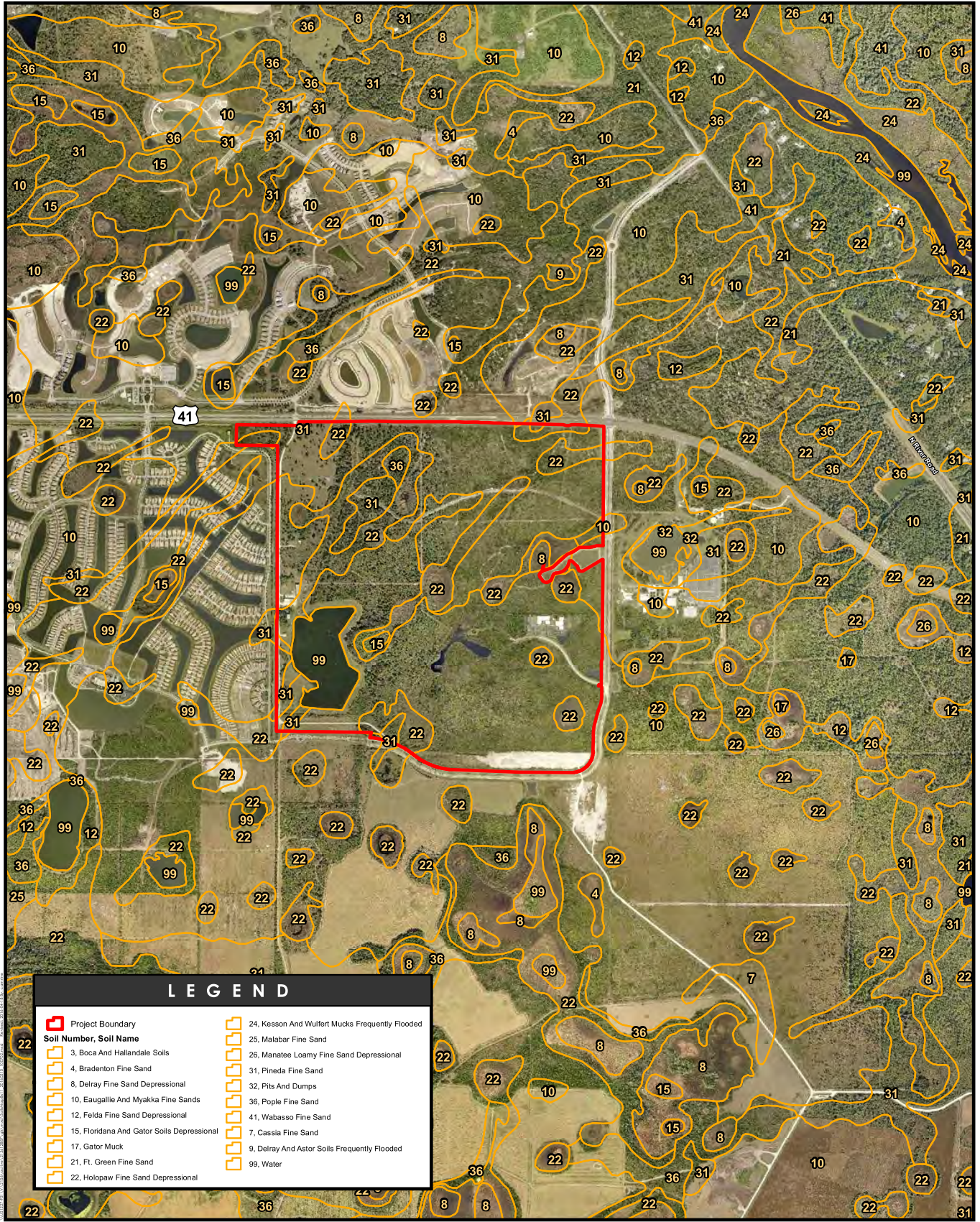
Project Location Map



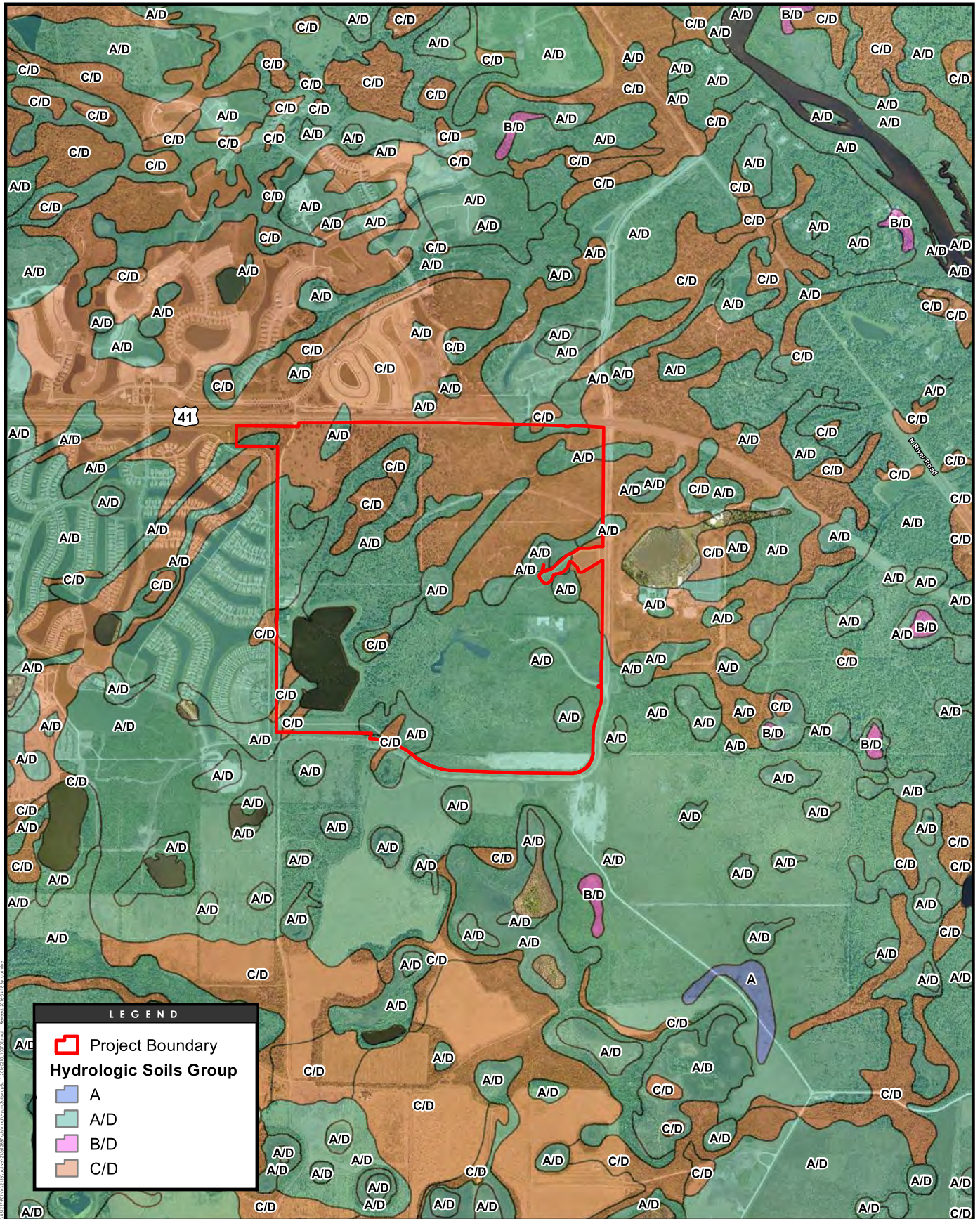
FEMA Floodplain Map



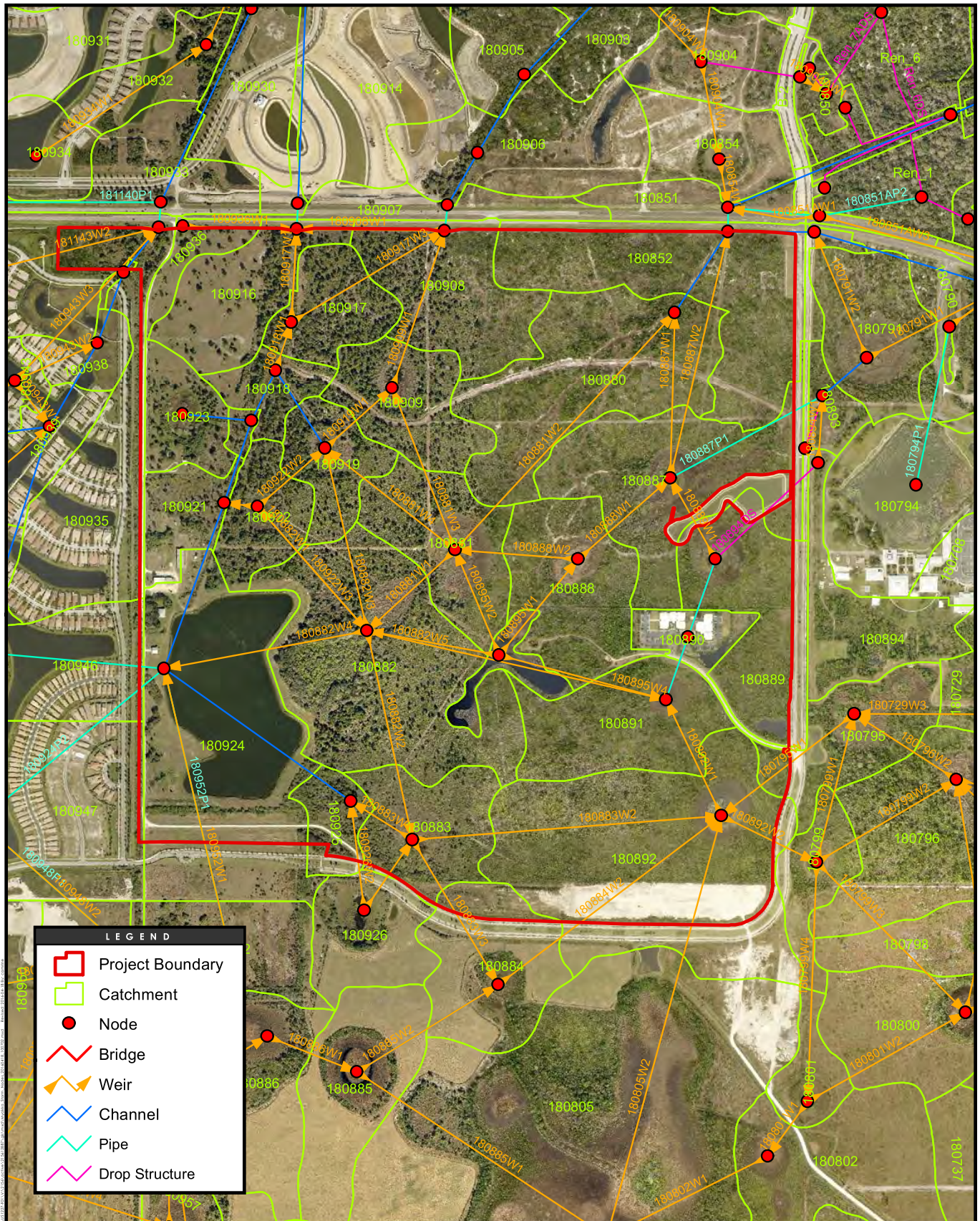
Sarasota County SCS Soil Map



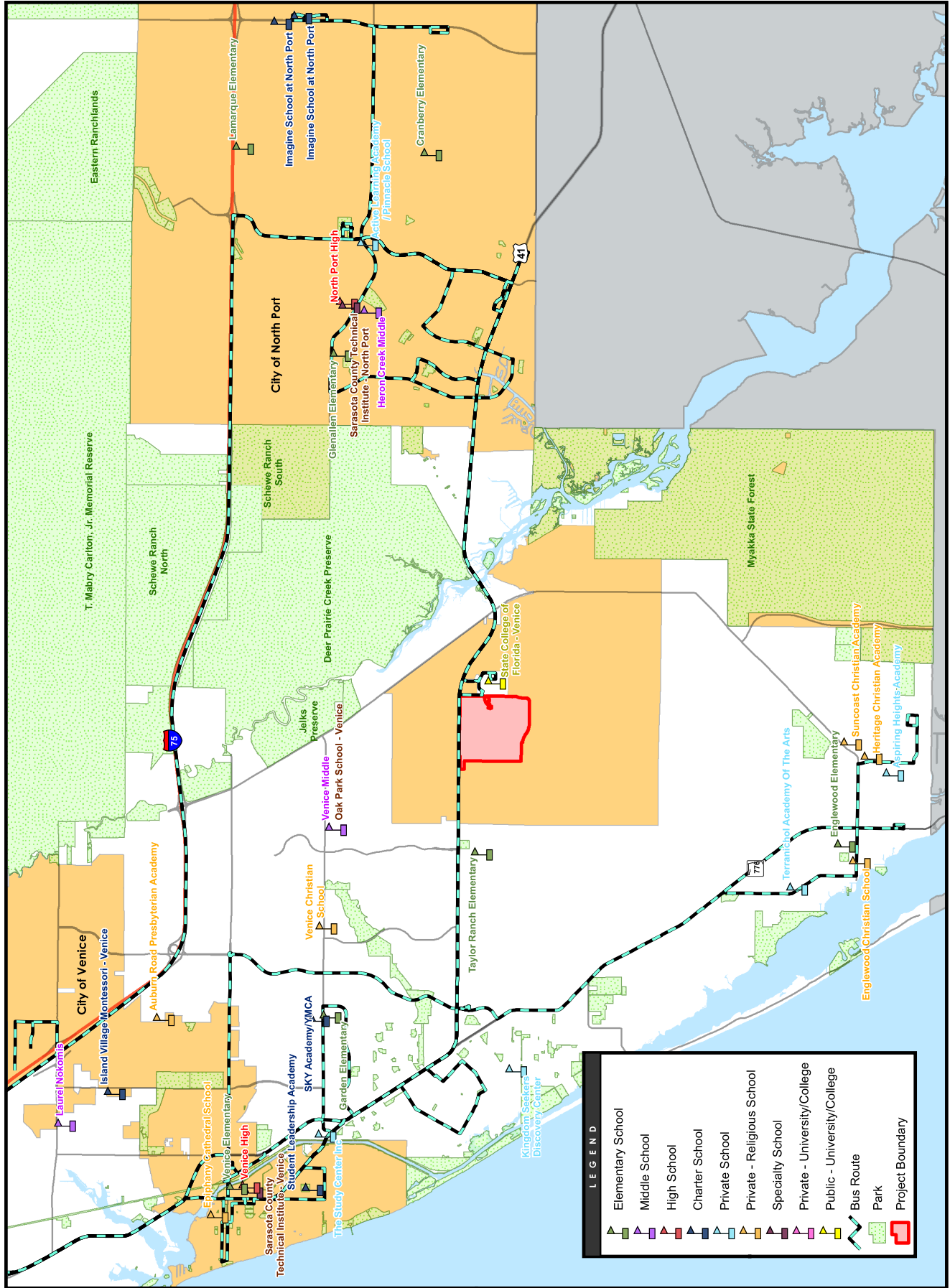
Sarasota County SCS Soil Map - Hydrologic Soil Group

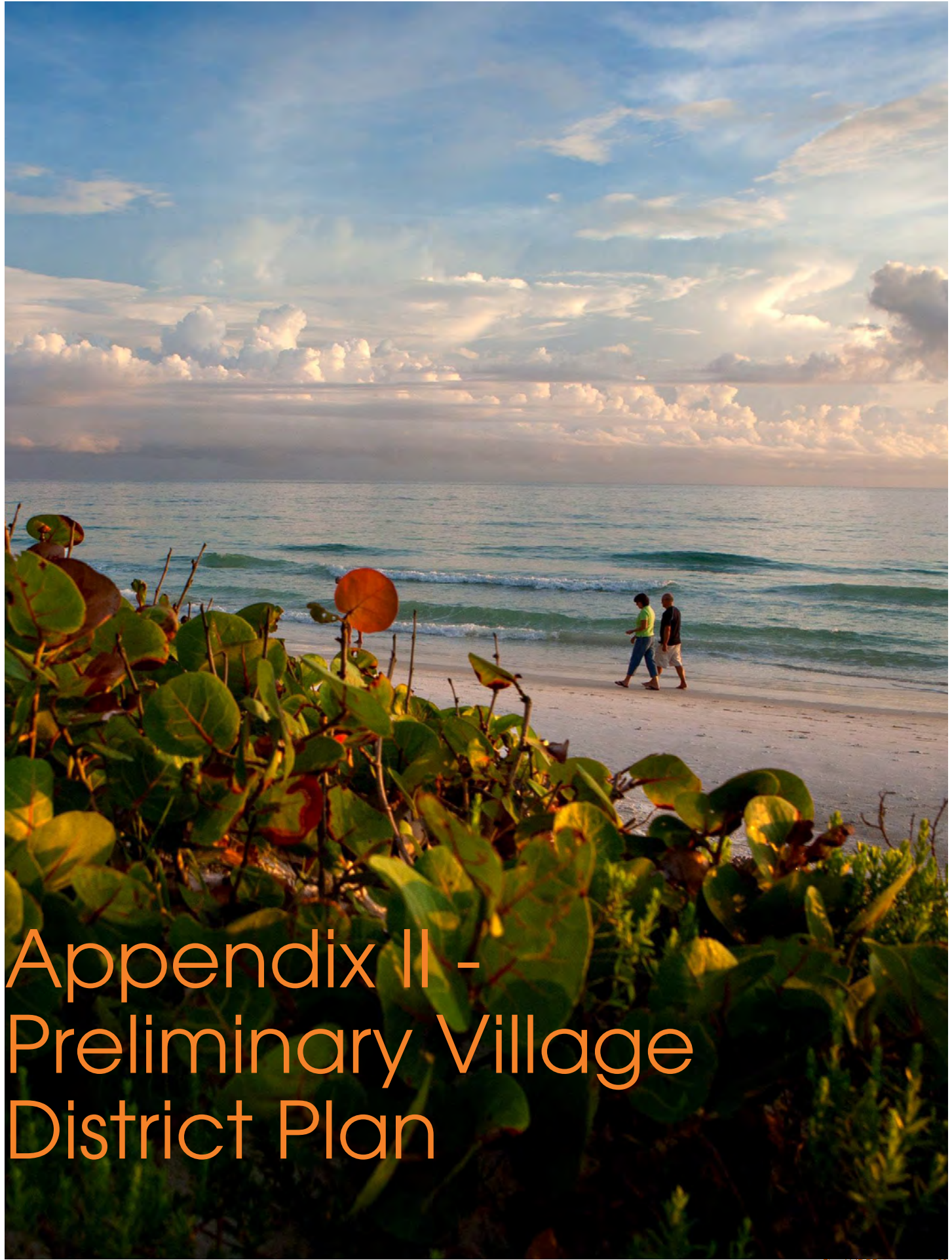


Myakka River Stormwater Model - Node Min/Max Report



Public Facilities Map - Schools, Bus Routes, and Park





Appendix II - Preliminary Village District Plan

Preliminary Village District Plan

2.1 Introduction

The Preliminary Village District Plan for Village “D” was designed according to the Village District Performance standards contained in the City of North Port’s Comprehensive Plan. The Site Analysis, as well as broader plans and ideas expressed in the Village District Pattern Book and Village Index Map have been relied upon as reference points for the development of this Preliminary Village District Plan.

Village “D” is envisioned as a vibrant mixed use Town Center that includes a broad mix of retail, commercial, hotel, recreational, office and residential uses. The Preliminary Village District Plan for Village “D” proposes three mixed use areas located primarily along the US 41 corridor, which will contain a broad mix of uses, and is envisioned as the primary focus of the more intensive commercial uses. The plan includes one mixed use residential neighborhood, which would include a mix of office, retail and residential uses. The plan also includes two residential neighborhoods, which would allow for a range of residential unit types. There is a purposeful transition in intensity from north to south. The transition is intended to ensure compatibility with the existing village of Islandwalk, to the west, as well as future development to the south and southeast.

The proposed Village Index Map includes a park within Village “D”. The location of the park is generally reflected on the Preliminary Village District Plan. Vehicular access to the park will be provided via the internal roadway network and the multi-modal trail system.

Heritage tree status will be determined during the Construction Plan process. If any additional Heritage Trees are identified, they will be addressed at that time, as well.

Additional Village characteristics including land uses, development styles, and other community features will be explained in further sections.

See Figure 2.1.A Preliminary Village District Plan

Neighborhoods

2.2 Neighborhood Character

Village “D” is intended to primarily serve as the Town Center for The West Villages, to include retail, office, civic and residential uses.

Three areas adjacent to US 41 are primarily envisioned for mixed use development, with a focus on retail and commercial uses. These areas are identified as MU-1, MU-2 and MU-3 on Figure 2.2.A – Preliminary Village Neighborhood Plan. The mixed use areas will primarily serve the commercial/retail and office needs of The West Villages. The proposed development standards are envisioned to permit a broad range of non-residential uses that will promote the long-term economic sustainability of the West Villages. Not only will this Town Center offer a variety of commercial and office development to meet the daily needs of residents, but it will also be made available for corporate centers and research and development offices or complexes, which will further the goals of the Village Land Use by encouraging a better jobs/housing balance. Finally, this neighborhood is proposed to offer a variety of residential unit types. Residential units will be intermixed, either vertically, or separately. Townhouses, stacked townhouses and multi-family uses may also be developed within the Mixed Use areas.

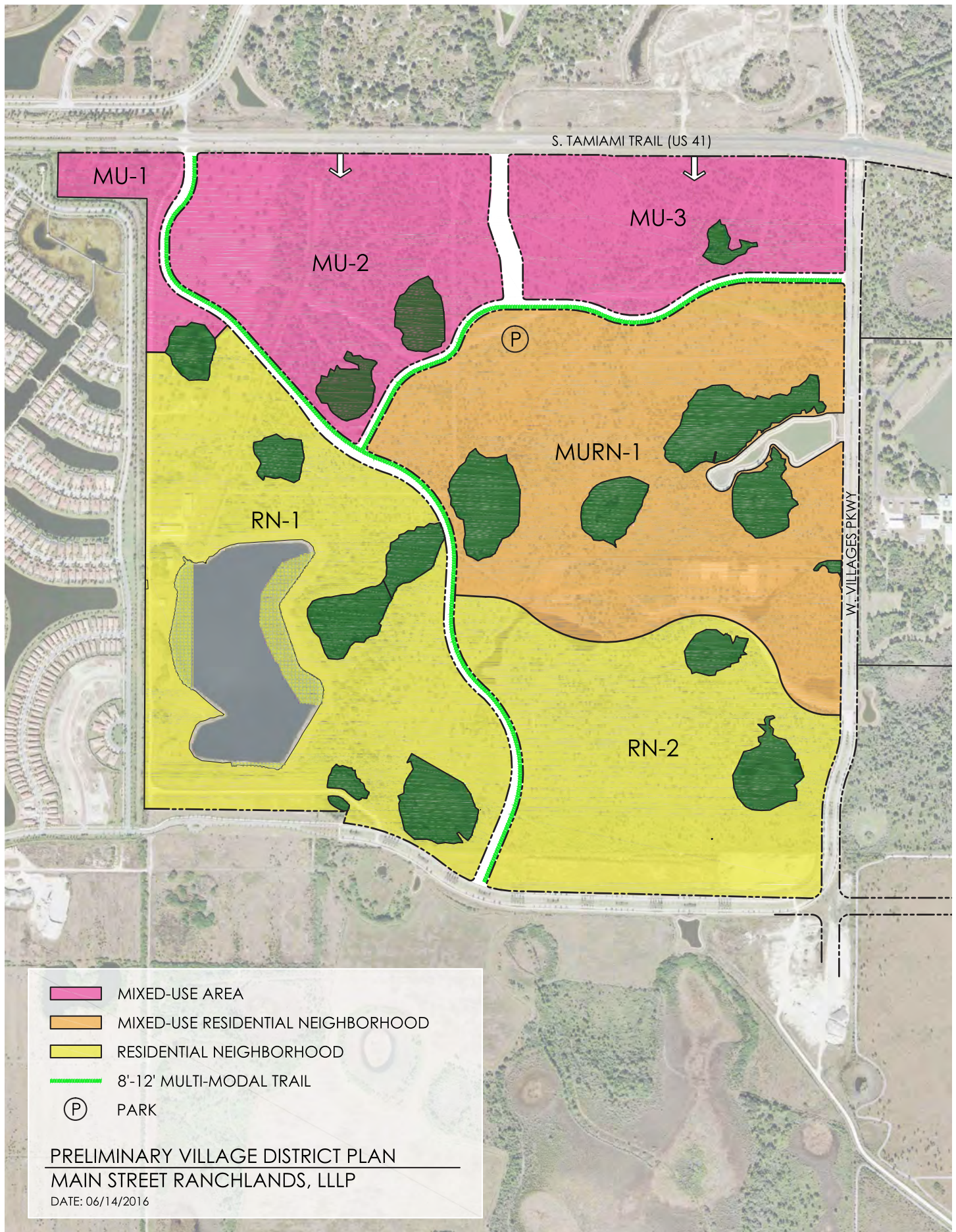
The Mixed Use Residential Neighborhood is envisioned as a neighborhood that contains a broad range of non-residential and residential land uses. Commercial/retail and office uses are also envisioned within this neighborhood, as well as multi-family, townhouses and single-family semidetached and detached units.

Two distinct moderate density residential neighborhoods, identified as RN-1 and RN-2, are also envisioned for Village “D”. The residential unit types, recreational amenities and preservation of natural and environmental features will enhance the character of these neighborhoods.

An important element of the village design includes a multi-modal trail network that connects all areas to neighborhood centers, open space tracts, as well as the Town Center, identified as MU-1, MU-2 and MU-3, as well as the park, generally identified on the Preliminary Village Neighborhood Plan. These features facilitate citizen interaction by linking village areas and neighborhoods to on-site and adjacent amenities and facilities. These trails will provide residents with recreational opportunities and access to the natural environment and open spaces.

This Village is designed to be a vibrant, mixed use area, offering a broad mix of residential unit types, which may be mixed vertically and horizontally with retail, commercial and office uses. The vision is to create a compact urban form in Mixed Use Areas 1-3, with a slight reduction in intensity in the Mixed Use Residential Neighborhood, to traditional moderate density residential neighborhoods, making available

Figure 2.1.A.



a range of housing types, in Residential Neighborhoods 1 and 2.

In furtherance of the interpretation authority granted by the City of North Port Comprehensive Plan and Land Development Code, the Zoning Administrator/Planning Manager shall have the authority to administratively approve modifications of standards and the conceptual design layout contained within this VDPP Application. Reasonable mitigation measures may be imposed by the Zoning Administrator/Planning Manager to limit impacts from the requested adjustment of standards. The Zoning Administrator/Planning Manager shall have the additional authority to administratively approve modifications to standards initiated by the property owner that provide a benefit to the general public or surrounding community, as long as those modifications do not increase density, maximum allowable building heights, reduce buffer widths along US 41, or add permitted uses. Standards contained herein may be met throughout the overall VDPP area and not individual parcels or lots.

In recognition of the critical role of the Town Center for the future economic development of the West Villages and the City of North Port, the following signage plan elements are established for the Village D VDPP to provide additional flexibility from those standards otherwise applicable through the City's Unified Land Development Code:

Landscape Standards:

To further the urban design character of the proposed Town Center and the desire to concentrate uses within a walkable, retail and entertainment district, canopy trees may be clustered based upon specific site designs to provide appropriate function and character to the space. Canopy trees will be utilized to provide natural shading within pedestrian environments and configured to allow sight lines to building facades and signage elements for the purposes of wayfinding. It is critical that trees placed within an urban environment have proper form at the time of planting. Given the variability of the plant nursery market, the Village D VDPP canopy trees will meet or exceed the criteria established by the current edition of the Florida Grades and Standards for Nursery Plants for a minimum of 2-inch caliper Florida #1 to provide greater flexibility in species and insure availability of structurally sound plant materials. (Figure 2.2.A Preliminary Village Neighborhood Plan)

2.3 Structure Types

Village "D" is planned to include a range of housing types, which will include Single-Family Detached, Single-

Family Semi-Detached, or Paired Villas, Single-Family Attached Townhouses with front or side-loaded garages, Single-Family Attached Townhouses with rear loaded garages, Stacked Townhouses, Multi-Family and residential units within Mixed-Use buildings. See Development Standards in Figure 2.3.A and 2.3.B for dimensional standards.

A Single-Family detached unit is a standalone house, not attached to any other dwelling by any means, and surrounded by open space or yards. Lot sizes vary, to allow a variety of usable private yard space and building separation. An accessory apartment is proposed as a permitted use on Single-Family Detached lots, which would allow an accessory dwelling unit with a separate means of ingress and egress containing a separate kitchen, bathroom and sleeping facilities that is either physically attached to, or contained within an existing single-family house or occupies the second story of an existing garage or accessory building on the same lot as the principal dwelling. Accessory apartments will not count as a dwelling unit for density calculation purposes.

A Single-Family Semi-detached unit is a one-family dwelling attached to one other one-family dwelling by a common vertical wall, with each dwelling located on a separate lot. Parcels contain one side yard on the opposite side of the common wall and a front and rear yard.

A Single-Family Attached Townhouse is a one-family dwelling in a row of at least three such units in which each unit has its own front and rear access to the outside, no unit is located over another unit, and each unit is separated from any other unit by one or more vertical common fire-resistant walls. Neighborhoods may contain Single-Family Attached units with front or side-loaded garages, or rear loaded garages, where an alley exists.

A stacked Townhouse typically has two units stacked vertically, with each having an entrance from the street.

A Multi-Family unit contains three or more dwelling units, which is typically two or more stories tall.

A Mixed-Use dwelling is one that is located within a mixed-use building, which contains a variety of complementary uses in a single building.

Figure 2.3.A Main Street Ranchlands Development Standards

Figure 2.3.B Typical lot configurations for individual residential structures

Figure 2.3.A Main Street Ranchlands Development Standards

	MIXED USE RESIDENTIAL NEIGHBORHOOD	RESIDENTIAL NEIGHBORHOOD	MIXED USE AREA
(+/-) ACRES	174	282	164
FLOOR AREA RATIO (3) DENSITY LIMITATIONS	2.0 FAR 24 Dwelling units per Acre	0.25 FAR 6 Dwelling units per Acre	3.0 FAR 36 Dwelling units per Acre
Permitted Uses(1)(5)(6)	Community Center, Gatehouse, Single-Family Detached Type A&B, Single-Family Semi-Detached, Townhouses, Stacked Townhouses, Multi-Family, Model Homes/Sales Center, Residential Support Uses, Retail, Commercial/Service, Parking/Utility/Communication/ Essential Services	Community Center, Gatehouse, Single Family Detached - Type A and Type B, Single Family Attached, Townhouses, Stacked Townhouses, Multi-Family, Model Homes/Sales Center, Residential Support Uses, Parking/Utility/Communication/ Essential Services	Community Center, Gatehouse, Single-Family Detached Type A&B, Townhouses, Stacked Townhouses, Multi-Family, Residential Support Uses, Retail, Commercial/Service, Parking/Utility/Communication/ Essential Services
Minimum Lot Size	See Figure 2.3.B.; Non-residential - no min. lot area	See Figure 2.3.B.; Non-residential - no min. lot area	See Figure 2.3.B.; Non-residential - no min. lot area
Maximum Structure Height	42 Feet (s.f.), 80 Feet (townhouses, community center, gatehouse), 120 feet (hotel/motel, multifamily, nonresidential)	42 Feet (s.f.), 60 Feet (townhouses, multi-family, community center, gatehouse, nonresidential)	50 Feet (s.f.) 60 feet (community center, gatehouse, townhouse), 120 Feet (multi-family, hotel/motel, non-residential)
Setbacks(2)(4)	Residential - See specific structure type in Section 2.3.B Non-residential(4) - Meet State Building & Fire Code	Residential - See specific structure type in Section 2.3.B. Non-residential(4) - Meet State Building and Fire Code	Residential - See specific structure type in Section 2.3.B Non-residential - Meet State Building and Fire Code

Notes:

- (1) Above ground utility structures shall be allowed anywhere within the Village provided that such facilities incorporate adequate levels of buffers to appropriately protect enjoyment on adjacent uses.
- (2) Fences, walls, columns, entry monumentation, decorative features, and utility facilities such as lift stations, storage tanks, ground mounted transformers and wells shall be exempt from any setback standards. A berm up to 8' in height may be constructed as part of a buffer. Up to 8' in height of wall or fence may be constructed with or without a berm as part of the landscape or buffering plans.
- (3) Floor to area ratio (FAR) standards shall be applied to individual phases in which a non-residential use is proposed.
- (4) Setbacks may be reduced to 0 feet when the subject parcel is adjacent to public/private right-of-way, easement, open space tract or water body that is at least 30 ft in width. Air conditioning units, pool pumps and other mechanical equipment shall be permitted in side yard setbacks no closer than 1.5' from lot line.
- (5) Utility structures may be located in easements or in right-of-ways as indicated in roadway cross-sections.
- (6) Lakes and ponds may be used for irrigation and or storage of reclaimed water.

Typical Configurations for Structures

The specific notes referenced below apply to the development standards outlined on the following pages for the structure types listed below.

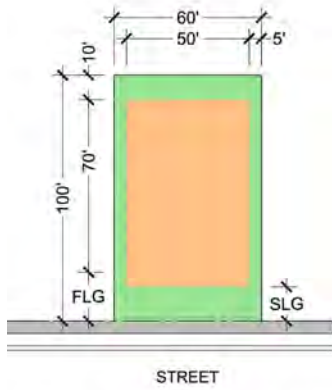
- Single-Family - Detached Type A
- Single-Family - Detached Type B
- Single-Family - Semi-detached Paired Villa
- Single-Family - Attached Townhome (Front Loaded)
- Single-Family - Attached Townhome (Rear Loaded)
- Multi-Family
- Mixed-Use

Development Standards	Notes
Lot Area Min.	9
Lot Width Min.	8,9
Lot Depth Min.	
Lot Coverage Max.	10
Front Setback Min. (Measured to sidewalk)	1
Side Setback Min. (Principal Structure)	3,4,7,8,11
Side Setback Min. (Pool Deck, Screen Enclosure)	2,3,7,8,11
Rear Setback Min. (Principle Structure)	5,7
Rear Setback Min. (Pool Deck, Screen Enclosure)	5,7

Notes

- 1 Front Loading Garage (FLG), Side Loading Garage (SLG), Recessed Garage Option (RGO)
- 2 Screen enclosures for Townhomes and Single-Family Attached (SFA) units will have a 5 FT side setback without a privacy wall, or a 0 FT side setback with a min. 6 FT high privacy wall, provided that the screen is located atop the wall.
- 3 Patios and pool decks for Townhomes and SFA units may have a 0 FT side setback provided units abut a shared privacy wall.
- 4 Corner setbacks do not apply when the side property line is adjacent to a platted open space tract of at least 5 FT in width.
- 5 The rear setback may be reduced to 0 FT when the rear property line abuts an easement, water body, or open space tract of at least 30 FT in dimension.
- 6 Townhome units may include two or more attached units.
- 7 Cornices, veneers, or other non-structural projections shall not count towards setbacks and shall be treated similar to roof overhangs.
- 8 Side yard setbacks for Single-Family Detached units are a combined 10 FT.
- 9 Minimum lot area and width for curvilinear lots may be less than required provided that all min. setback requirements are met and the average lot width (front lot line and rear lot line) is equal to or greater than the min. lot width required. Irregular lot dimensions that meet minimum lot size are permitted.
- 10 Lot coverage is defined as the percent of lot area under fixed roof. Lot coverage does not include pools, decks, driveways, patios, sidewalk, etc.
- 11 Air-conditioning units and mechanical equipment shall be allowed in side yard setbacks no closer than 1.5' from lot line.
- 12 For Single-Family Detached type A and B, A is provided for illustrative purposes. B reflects the minimum dimensional standards.
- 13 FLG setback is to the garage face. SLG and RGO setback minimum shall apply to the non-garage portion of the structure.

Figure 2.3.C.



Development Standards

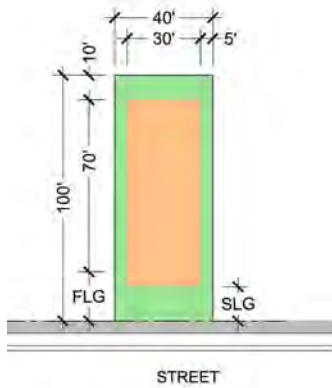
- Lot Area Min.
- Lot Width Min.
- Lot Depth Min.
- Lot Coverage Max.
- Front Setback Min. (Measured to sidewalk)
- Side Setback Min. (Principal Structure)
- Side Setback Min. (Pool Deck, Screen Enclosure)
- Rear Setback Min. (Principle Structure)
- Rear Setback Min. (Pool Deck, Screen Enclosure)

Single-Family Detached (Type-A)

- 6,000 SF
- 60 FT
- 80 FT
- 65%
- 20 FT (FLG) / 10 FT (SLG)(RGO)
- 10 FT Between Structures, 10 FT Corner Lot
- 3 FT Pool; 0' Pool Deck/Screen
- 10 FT
- 0 FT (deck/screen) / 3 FT (Pool Edge)

SINGLE-FAMILY

DETACHED - TYPE A



Development Standards

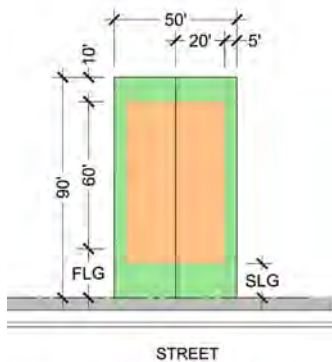
- Lot Area Min.
- Lot Width Min.
- Lot Depth Min.
- Lot Coverage Max.
- Front Setback Min. (Measured to sidewalk)
- Side Setback Min. (Principal Structure)
- Side Setback Min. (Pool Deck, Screen Enclosure)
- Rear Setback Min. (Principle Structure)
- Rear Setback Min. (Pool Deck, Screen Enclosure)

Single-Family Detached (Type-B)

- 4,000 SF
- 40 FT
- 80 FT
- 65%
- 20 FT (FLG) / 10 FT (SLG)(RGO)
- 10 FT Between Structures, 10 FT Corner Lot
- 3 FT Pool; 0 FT Pool Deck/Screen
- 10 FT
- 0 FT (Pool Deck) / 3 FT (Pool Edge)

SINGLE-FAMILY

DETACHED - TYPE B



Development Standards

- Lot Area Min.
- Lot Width Min.
- Lot Depth Min.
- Lot Coverage Max.
- Front Setback Min. (Measured to sidewalk)
- Side Setback Min. (Principal Structure)
- Side Setback Min. (Pool Deck, Screen Enclosure)
- Rear Setback Min. (Principle Structure)
- Rear Setback Min. (Pool Deck, Screen Enclosure)

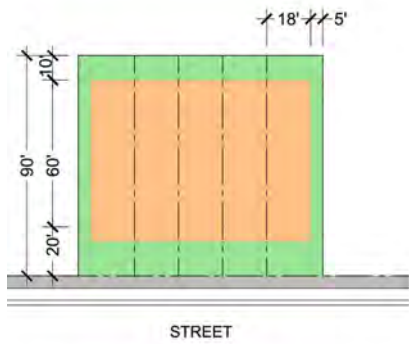
Single-Family Semi-detached (Paired Villa)

- 2,250 SF (per unit)
- 25 FT
- 70 FT
- 75%
- 20 FT (FLG) / 10 FT (SLG)(RGO)
- 10 FT Between Structures (0 FT Common Wall/Shared Lot Line) / 10 FT Corner Lot
- 3 FT (0 FT Common Wall/Shared Lot Line) / 5 FT (Side Yard Pool Edge)
- 10 FT
- 0 FT (deck/screen) / 3 FT (Pool Edge)

SINGLE-FAMILY

SEMI-DETACHED - PAIRED VILLA

Figure 2.3.D.



Development Standards

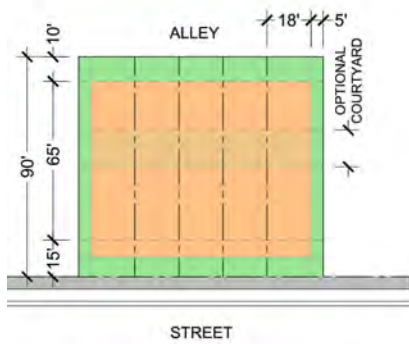
- Lot Area Min.
- Lot Width Min.
- Lot Depth Min.
- Lot Coverage Max.
- Front Setback Min. (Measured to sidewalk)
- Side Setback Min. (Principal Structure)
- Side Setback Min. (Pool Deck, Screen Enclosure)
- Rear Setback Min. (Principle Structure)
- Rear Setback Min. (Pool Deck, Screen Enclosure)

Single-Family Attached (Townhome) (6)

- 1,620 SF (per unit)
- 18 FT
- 85 FT
- 75%
- 20 FT / 10 FT (Porch/Stoop) (SLG)(RGO)
- 10 FT Between Structures (0 FT Common Wall/Shared Lot Line) / 10 FT Corner Lot
- 3 FT Pool (0 FT Common Wall/Shared Lot Line) / 3 FT (Side Yard Pool Edge/Screen)
- 15 FT
- N/A

SINGLE-FAMILY

ATTACHED - TOWNHOME (FRONT-LOADED)



Development Standards

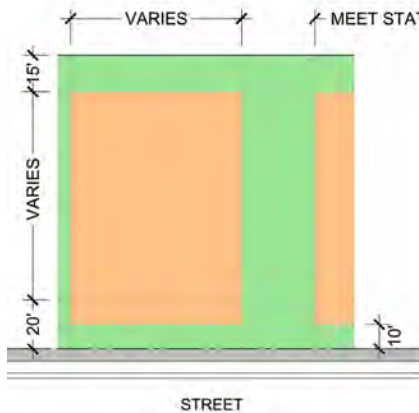
- Lot Area Min.
- Lot Width Min.
- Lot Depth Min.
- Lot Coverage Max.
- Front Setback Min. (Measured to sidewalk)
- Side Setback Min. (Principal Structure)
- Side Setback Min. (Pool Deck, Screen Enclosure)
- Rear Setback Min. (Principle Structure)
- Rear Setback Min. (Pool Deck, Screen Enclosure)

Single-Family Attached (Townhome) (6)

- 1,620 SF (per unit)
- 18 FT
- 60 FT
- 75%
- 15 FT / 8 FT (Porch/Stoop)
- 5 FT (0 FT Common Wall/Shared Lot Line) / 10 FT Corner Lot
- 4 FT (0 FT Common Wall/Shared Lot Line) / 5 FT (Side Yard Pool E
- 10 FT
- N/A

SINGLE-FAMILY

ATTACHED - TOWNHOME (ALLEY-LOADED)



Development Standards

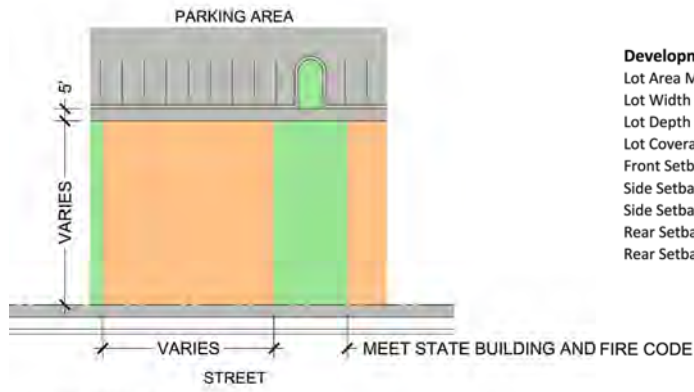
- Lot Area Min.
- Lot Width Min.
- Lot Depth Min.
- Lot Coverage Max.
- Front Setback Min. (Measured to sidewalk)
- Side Setback Min. (Principal Structure)
- Side Setback Min. (Pool Deck, Screen Enclosure)
- Rear Setback Min. (Principle Structure)
- Rear Setback Min. (Pool Deck, Screen Enclosure)

Multi-Family

- N/A
- N/A
- N/A
- N/A
- 20 FT / 10 FT (SLG)(RGO)
- Meet State Building and Fire Code
- N/A
- 15 FT
- N/A

MULTI-FAMILY/STACKED TOWNHOUSE

Figure 2.3.E.



Development Standards

- Lot Area Min.
- Lot Width Min.
- Lot Depth Min.
- Lot Coverage Max.
- Front Setback Min. (Measured to sidewalk)
- Side Setback Min. (Principal Structure)
- Side Setback Min. (Pool Deck, Screen Enclosure)
- Rear Setback Min. (Principle Structure)
- Rear Setback Min. (Pool Deck, Screen Enclosure)

Mixed-Use

- N/A
- N/A
- N/A
- N/A
- 0 FT
- Meet State Building and Fire Code
- N/A
- N/A
- N/A

MIXED-USE

RESIDENTIAL / NON-RESIDENTIAL

Neighborhood Centers

2.4 Neighborhood Centers

Neighborhood centers are intended to serve residential neighborhoods and may provide recreation, neighborhood scale commercial, office, or civic uses. Village “D” anticipates that each of the neighborhood centers will maintain the character of the neighborhood, and will offer a range of uses to create variety and function within the village.

Neighborhood centers in Residential Neighborhoods 1 and 2 may be designed as amenity centers, passive or active parks, neighborhood greens, or civic nodes. These neighborhoods will be designed, so homes are generally within a 1/2 mile radius of the neighborhood center.

Consistent with the proposed Index Map, Mixed Use Residential Neighborhood-1 includes a park that will have both active and passive recreational opportunities for residents of The West Villages. Vehicular access will be provided from all areas to this park. It will also be accessible by residents via the multi-modal trail.

and Pathways depict improvements that are impact fee and mobility fee creditable, and are to be constructed and maintained by the WVID and/or the Master Developer.

Section 2.6.1

Main Streets (Figure 2.6.1.A-B) are generally slow movement local roadways that offer a pedestrian environment and on-street parking for the commercial Main Street. Certain areas also provide for free movement, at intersections to arterials. These roads are intended to provide connectivity to other Villages and arterials.

Town Center

2.5 Town Center

Village “D” contains three areas dedicated to Town Center development, which will offer residents of The West Villages access to a broad range of retail/commercial, office, civic, and residential uses. To insure commercial development does not exceed market demand, no Village Centers are proposed within Village “D”.

Roadways and Pathways

2.6 Roadways and Pathways

Roadways within Village “D” will comfortably accommodate vehicular, pedestrian, Neighborhood Electric Vehicles and bicycle traffic. Through design and development of Village “D”, the proposed maximum posted speed will be up to 35 miles per hour, to encourage the use of Neighborhood Electric Vehicles as an alternative mode of transportation for trips within the West Villages. A sidewalk system will be constructed to facilitate pedestrian circulation. In addition, roadways will be landscaped and lit to enhance the community appearance and contribute to pedestrian comfort. Described below are six types of roadways and pathways that can be implemented for the village development: Main Streets, Mixed-Use Streets and Avenues. Typical cross sections are depicted in the subsequent sections. The Roadways

Section 2.6.1.

Main Streets (Figure 2.6.1.A-B) are generally slow movement local roadways that offer a pedestrian environment and on-street parking for the commercial Main Street. Certain areas also provide for free movement, at intersections to arterials. These roads are intended to provide connectivity to other Villages and arterials.

Figure 2.6.1.A.

Main Street At US41 / Boulevard (Preto)

Main Street as it intersects US Highway 41 is a free movement thoroughfare as it enters a full access intersection. Preto Boulevard is a 4 lane divided thoroughfare.

- Free movement thoroughfare as it enters a full access intersection.
- Typically a two- to four-lane divided roadway with turn lanes.
- Provides regional connections to the other villages and arterials.
- May be designed for up to 25 mile per hour speed limit.
- May include trees and landscaping in median and public frontages.
- Designed to accommodate pedestrian, bicycle, and small electric-powered vehicles.

Design Parameters	Min.	Max.
Target Speed	15	35
Travel Way Dimensions	Min.	Max.
Travel Lane(s) Width (ft)	11	14
Turn Lane Width	10	12
Bike Lane Width (ft)	5	7 (Buffered)
Parking Lane Width (Parallel Parking) (ft)	N/A	N/A
Parking Lane Width (Angled Parking) (ft)	N/A	N/A
Public Frontage	Min.	Max.
Planting Type	Planted/Grass	
Planting Width (ft)	0	-
Walkway Width (each side) (ft)	5	-
Right Of Way	Min.	Max.
ROW Width	100	-

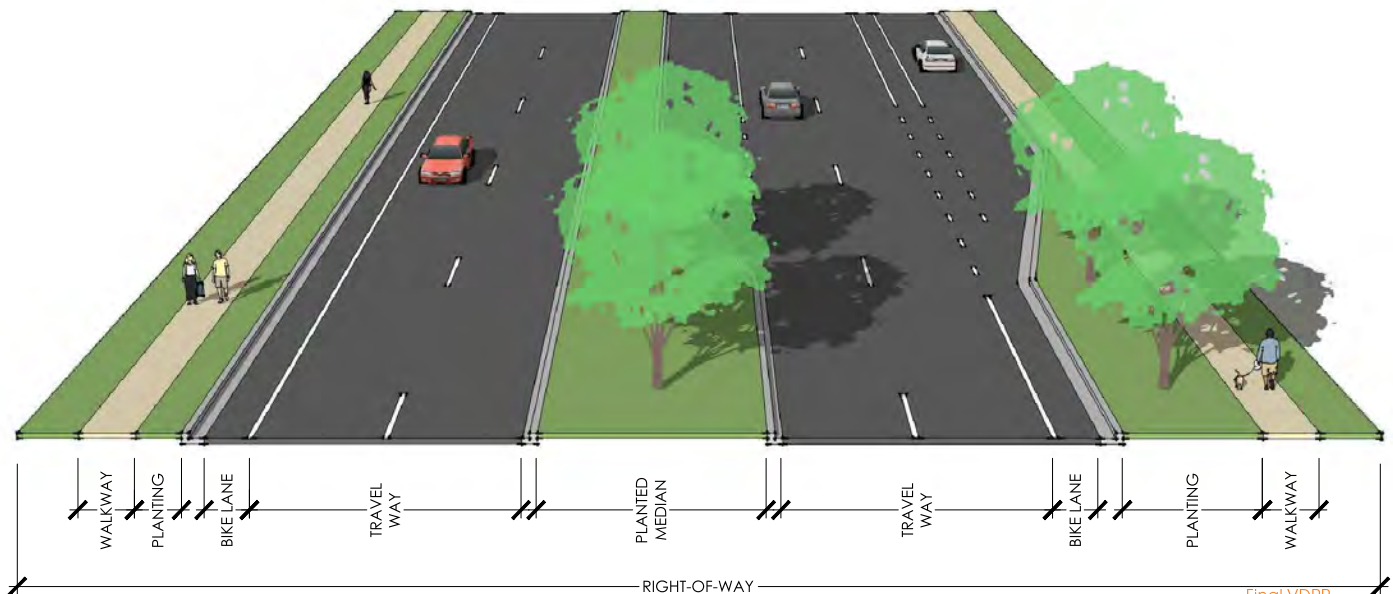


Figure 2.6.1.B.

Main Street

Main Street is a slow movement local roadway providing a pedestrian environment and on-street parking for the commercial main street.

- Slow movement local roadway that offers a pedestrian environment for the commercial Main Street.
- Typically a two-lane divided roadway with angled or parallel parking on either side of the travel lane.
- May be designed for 25 mile per hour speed limit.
- May provide for public frontages with plantings containing trees.
- Provides mid-block un-signalized walkways to promote pedestrian connectivity.
- Designed to accommodate pedestrian, bicycle and small electric-powered vehicles.

Design Parameters	Min.	Max.
Target Speed	15	25
Travel Way Dimensions	Min.	Max.
Travel Lane(s) Width (ft)	12	17
Parking Lane Width (Parallel Parking) (ft)	8	9
Parking Lane Width (Angled Parking) (ft)	16	19
Public Frontage	Min.	Max.
Planting Type	Intermittent	
Planting Width (ft)	0	6
Walkway Width (ft)	10	-
Right Of Way	Min.	Max.
ROW Width	120	200

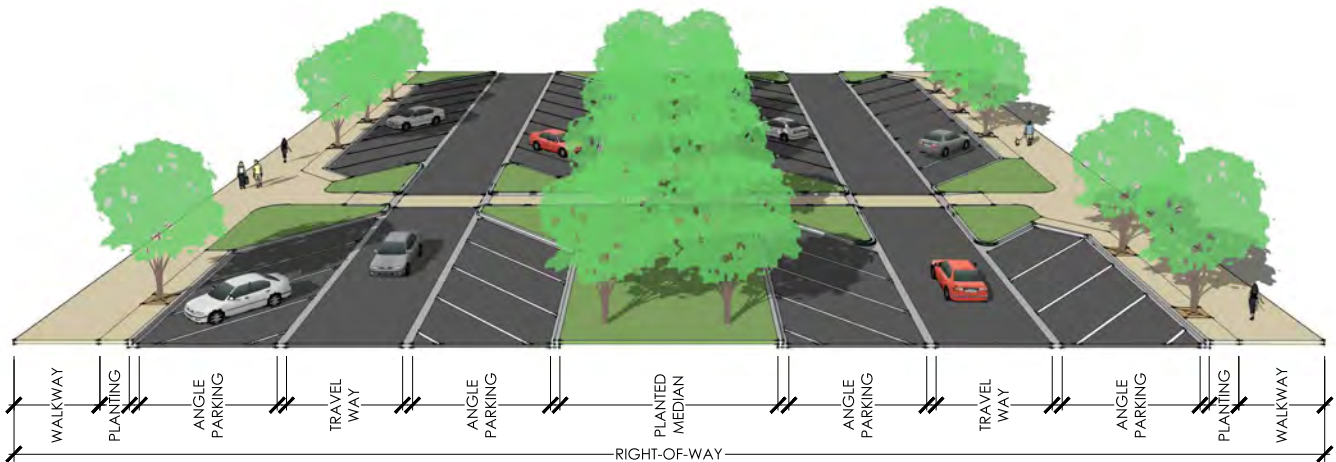


Figure 2.6.1.C.

Mixed Use Street A

Mixed Use Streets are a free movement local roadway providing on-street parking for higher intensity uses including shops, offices, multi-family buildings, and townhomes.

- Free movement local roadway providing on-street parking for higher intensity uses including shops, offices, multi-family buildings, and Townhouses.
- Typically a two-lane undivided roadway.
- May be designed for 25 mile per hour speed limit.
- Designed for on-street parking (angled or parallel).
- Public frontages may include landscaped planters with vegetation.
- Designed to accommodate pedestrian, bicycle connectivity, and small electric-powered vehicles.

Design Parameters	Min.	Max.
Target Speed	15	25
Travel Way Dimensions	Min.	Max.
Travel Lane(s) Width (ft)	11	12
Bike Lane Width (ft)	5	7 (Buffered)
Optional Parking Lane Width (Parallel Parking) (ft)	8	9
Parking Lane Width (Angled Parking) (ft)	N/A	N/A
Public Frontage	Min.	Max.
Planting Type		Intermittent
Planting Width (ft)	-	6
Walkway Width (ft)	5	-
Right Of Way	Min.	Max.
ROW Width	60	90

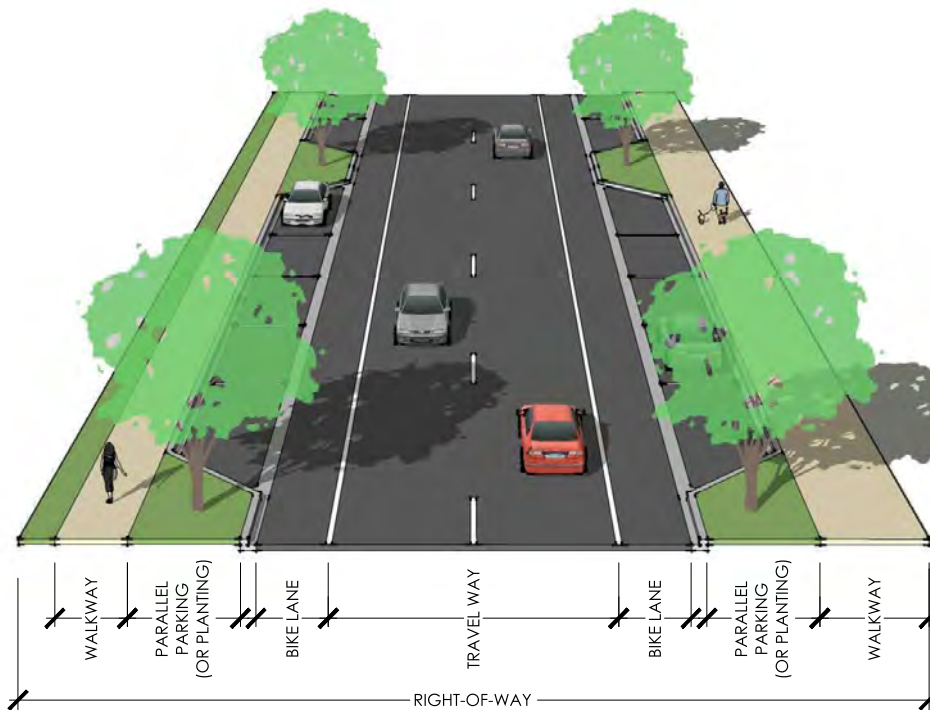


Figure 2.6.1.D.

Mixed Use Street B

Mixed Use Streets are a free movement local roadway providing frontage and on-street parking for higher density uses including shops, offices, multi-family buildings, and townhomes.

- Free movement local roadway.
- Provides frontage and on-street parking for higher density uses including shops, offices, multi-family buildings, and Townhouses.
- Typically a two-lane undivided roadway.
- Accommodates parallel parking.
- May be designed for 25 mile per hour speed limit.
- Designed to accommodate pedestrian connectivity with sidewalks, separated from vehicular traffic with landscaping, as well as bicycles and small electric-powered vehicles.

Design Parameters	Min.	Max.
Target Speed	15	25
Travel Way Dimensions	Min.	Max.
Travel Lane(s) Width (ft)	11	12
Bike Lane Width (ft)	N/A	N/A
Optional Parking Lane Width (Parallel Parking) (ft)	8	9
Optional Parking Lane Width (Angled Parking) (ft)	N/A	N/A
Public Frontage	Min.	Max.
Planting Type	Intermittent	
Planting Width (ft)	4	-
Walkway Width (ft)	5	-
Right Of Way	Min.	Max.
ROW Width	54	80

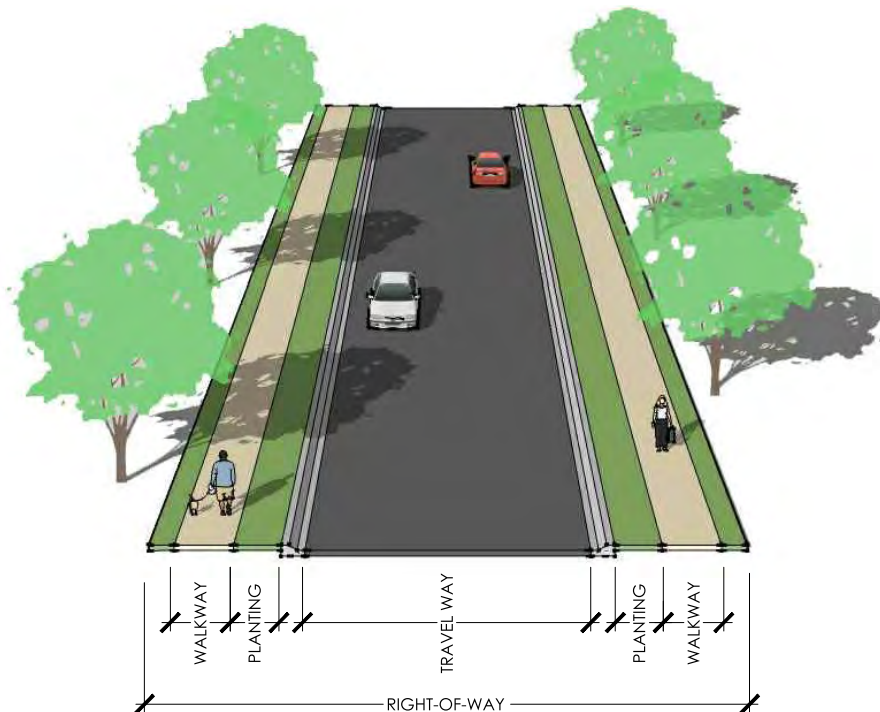


Figure 2.6.1.E.

Avenue

Avenues are free movement local roadways providing circulation through and connecting locations within the community.

- Frequently used roadways, intended to connect neighborhoods to one another.
- Typically, a two-lane undivided roadway.
- Designed to accommodate pedestrian, bicycle and small electric-powered vehicles (may be a trail).
- May be designed for 35 mile per hour speed limit.
- May be lined with canopy trees.

Design Parameters	Min.	Max.
Target Speed	25	35
Travel Way Dimensions		
Travel Lane(s) Width (ft)	11	12
Optional Bike Lane Width (ft)	4	5
Parking Lane Width (Parallel Parking) (ft)	N/A	N/A
Parking Lane Width (Angled Parking) (ft)	N/A	N/A
Public Frontage		
Planting Type	Planted	
Planting Width (ft)	4	-
Optional Walkway Width (ft)	5	-
Multi-Modal Trail Width (ft)	8	12
Right Of Way		
ROW Width	70	100

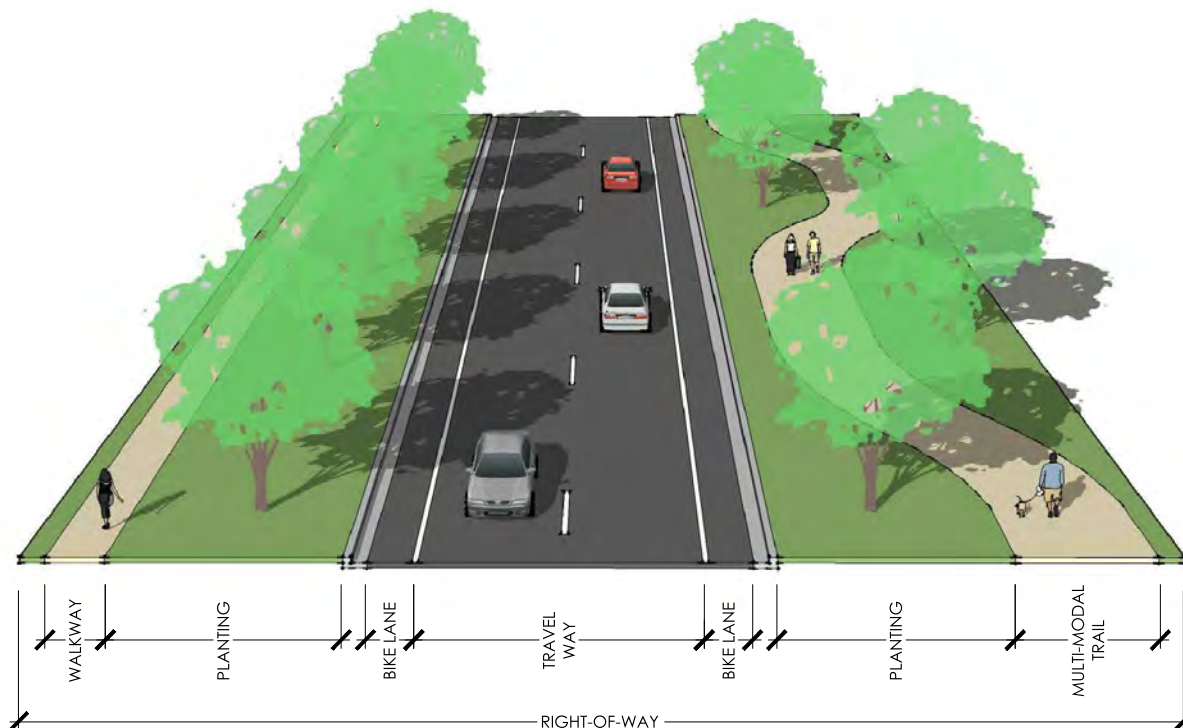


Figure 2.6.1.F.

Avenue At Intersection

Avenues are free movement local roadways providing circulation through and connecting locations within the community.

- Frequently used roadways, intended to connect neighborhoods to one another.
- Typically a two-lane undivided roadway.
- Designed to accommodate pedestrian, bicycle and small electric-powered vehicles (may be a trail).
- May be designed for 35 mile per hour speed limit.
- Lined with canopy trees.
- Turn lanes are included to accommodate safe turning movements.

Design Parameters	Min.	Max.
Target Speed	25	35
Travel Way Dimensions		
Travel Lane(s) Width (ft)	11	12
Turn Lane Width (ft)	10	12
Optional Bike Lane Width (ft)	4	5
Parking Lane Width (Parallel Parking) (ft)	N/A	N/A
Parking Lane Width (Angled Parking) (ft)	N/A	N/A
Public Frontage		
Planting Type	Planted	
Planting Width (ft)	4	-
Optional Walkway Width (ft)	5	-
Multi-Modal Trail Width (ft)	8	12
Right Of Way		
ROW Width	90	120

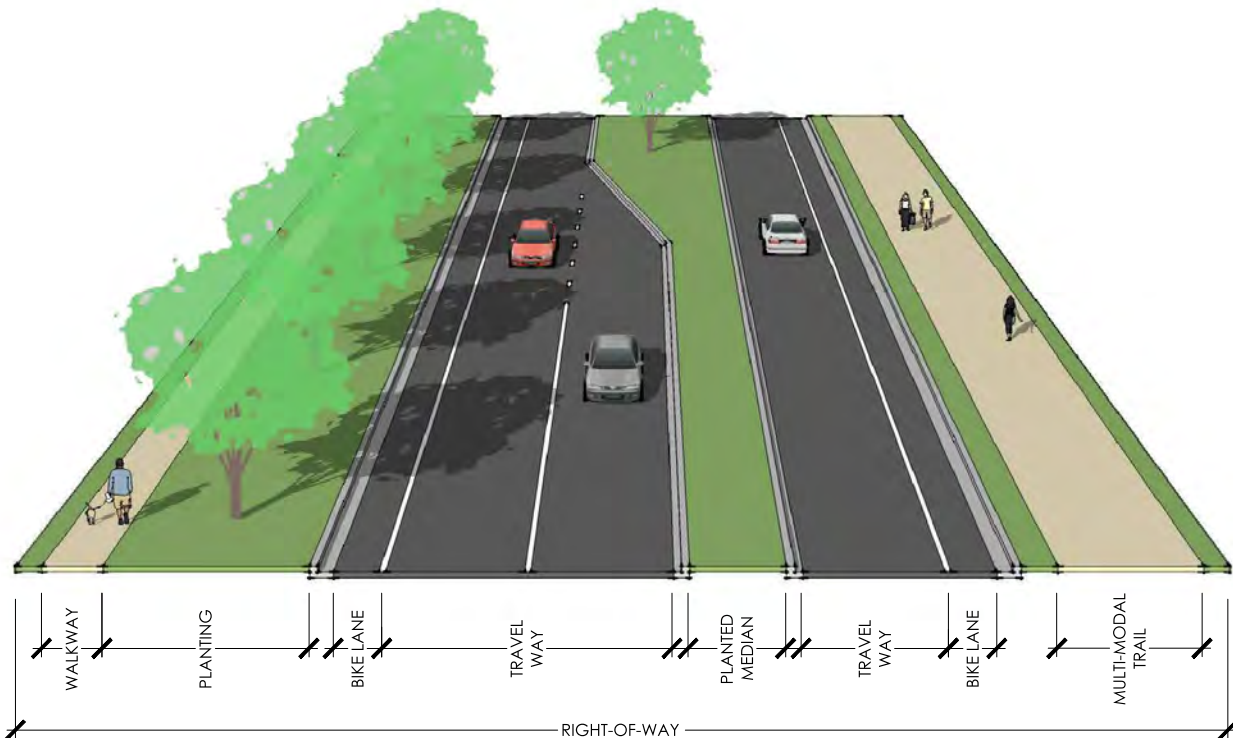


Figure 2.6.1.G.

Residential Neighborhood Street

Residential Neighborhood Streets are small scale, slow movement local roadways suitable for neighborhood areas.

- Small scale, slow movement local roadways suitable for neighborhoods.
- Typically two lanes, undivided.
- Link neighborhoods and neighborhood centers to avenues.
- May be designed for speed limits of up to 25 miles per hour, typically posted lower.
- Accommodates neighborhood vehicles, pedestrians, bicycles and small electric-powered vehicles.

Design Parameters	Min.	Max.
Target Speed	15	25
Travel Way Dimensions	Min.	Max.
Travel Lane(s) Width (ft)	10	12
Turn Lane Width (ft)	N/A	N/A
Bike Lane Width (ft)	N/A	N/A
Parking Lane Width (Parallel Parking) (ft)	N/A	N/A
Parking Lane Width (Angled Parking) (ft)	N/A	N/A
Public Frontage	Min.	Max.
Planting Type	Planted	
Planting Width (ft)	4	-
Walkway Width (ft)	5	-
Right Of Way	Min.	Max.
ROW Width	40	60



NOTE: Sidewalk only required along frontage of residential units.

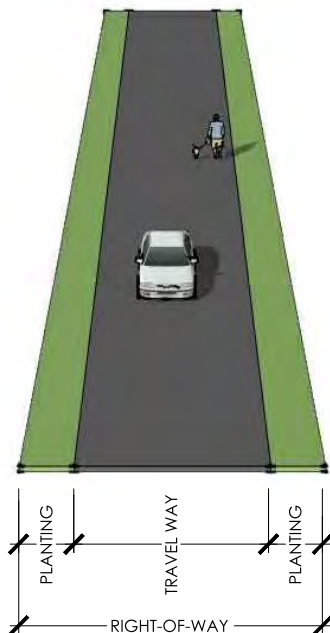
Figure 2.6.1.H.

Residential Neighborhood Alley

Residential Neighborhood Alleys are small scale, slow movement drives for rear access to homes.

- Provide access to rear loading garages or parking areas.
- Designed with 10 foot travel lanes.
- Designed as one-way sections.
- Intended for “traditional neighborhood designs” in neighborhoods.
- Intended to provide access to rear of commercial/mixed use buildings in Mixed Use areas and Mixed Use Residential Neighborhood.

Design Parameters	Min.	Max.
Target Speed	N/A	N/A
Travel Way Dimensions	Min.	Max.
Travel Lane(s) Width (ft)	10	16
Turn Lane Width (ft)	N/A	N/A
Bike Lane Width (ft)	N/A	N/A
Parking Lane Width (Parallel Parking) (ft)	N/A	N/A
Parking Lane Width (Angled Parking) (ft)	N/A	N/A
Public Frontage	Min.	Max.
Planting Type	N/A	N/A
Planting Width (ft)	N/A	N/A
Walkway Width (ft)	N/A	N/A
Right Of Way	Min.	Max.
ROW Width	15	25



Infrastructure

2.7 Infrastructure

Section 2.6.2

Mixed-Use Streets are generally free movement local roadways that provide on-street parking for higher density areas, including shops, offices, multi-family buildings, and Townhouses..

Section 2.6.3

Avenues are frequently used roadways, intended to connect neighborhoods to one another. They are typically free movement local roadways that provide circulation and connectivity to other areas of the community. Avenues are intended to divert higher traffic volumes away from residential neighborhoods while allowing for interconnectivity within the Village.

Section 2.6.4

Residential Neighborhood Streets are roadways intended for use in residential neighborhoods. They are intended to accommodate neighborhood vehicles, pedestrians, bicycles and electric vehicles They are typically two lane, undivided roads intended to link neighborhoods and neighborhood centers to avenues.

Sections 2.6.5

Alleys are narrow roadways, typically located adjacent to the rear of lots, intended to provide access to rear loading garages or parking areas. They are typically designed with 10 foot travel lanes, and are intended for one-way traffic.

Section 2.6.6

Multi-Modal Trails are designed in designated rights-of-way. They are intended to be 8 to 12 foot trails that blend with surrounding neighborhoods and neighborhood centers through the use of landscaping and native vegetation and trees that are consistent with surrounding areas. Finally, the surface of the trail may vary from paved to other materials (i.e. mulch, shell, etc.).

Preliminary provisions have been made for water, wastewater, stormwater and solid waste as required by the City of North Port Unified Land Development Code. The West Villages are already subject to the Principles of Agreement addressing the provision of major infrastructure to serve the existing and proposed villages. The Principles of Agreement and Utility Agreement serve to meet the Developers Agreement requirements pursuant to Chapter 54 ULDC.

Water, sewer, reuse and irrigation infrastructure is available to the property. Formal negotiations are underway to ensure availability concurrent with development through mains in adjacent roadways. The agreement to accommodate long-and short-term service was formally initiated through a memorandum dated September 18, 2000 regarding water and sewer availability for the West Villages. The City's memorandum includes the anticipated responsibilities of both the City and the West Village Improvement District for providing utility service during interim and final build out periods for the area. At present, the City has water and sewer infrastructure capacity to serve the subject site; however, additional utility infrastructure is anticipated, and includes water and wastewater treatment infrastructure, intended to provide service to this and other Villages. The West Villages Improvement District (WVID) was formed to provide a mechanism to construct the utility infrastructure required to serve new development within the West Villages.

Section 2.7.1 Water and Wastewater

At this time, water and sewer infrastructure sufficient to serve portions of Village "D" will be provided through an existing water main and force main that connect to existing City plants. A potable water pump station and storage tank has been constructed to ensure adequate pressure during peak domestic and fire demand.

It is anticipated water for irrigation will be supplied through wells and surface waters as permitted by water use permits and may be supplemented by reclaimed water. The long-term service requirements of the West Villages are planned to be served with centralized water and sewer systems, as well as a dispersed irrigation quality water system. It is anticipated that new water and wastewater treatment infrastructure will be co-located and constructed within the West Villages. The WVID will be required to design and construct water and wastewater treatment infrastructure, then ultimately turn them over to the City to own and operate.

Section 2.7.2 Stormwater Management

Stormwater will be retained in a system of lakes within the village. Ideally, the lakes will serve the village as a whole, including the individual areas and neighborhoods. The stormwater lakes are being sized to effectively accommodate stormwater demand for residential and non-residential uses. These features will serve as a community amenity. Specific lake size and topographic alterations will be developed as part of the next steps of the Village District planning process and construction plan development.

Section 2.7.3 Solid Waste

Solid waste is expected to be collected by the City of North Port Solid Waste District. Preliminary plans allow collection vehicles to enter the community and collect waste either from dumpster enclosures for non-residential or mixed use buildings, and from individual units in neighborhoods. Recycling services are provided on a voluntary basis within the City. The City of North Port has not identified any deficiencies in solid waste capacity.

Figure 2.7.A Preliminary Water System Plan

Figure 2.7.B Preliminary Wastewater System Plan

Figure 2.7.A.
Water System Plan

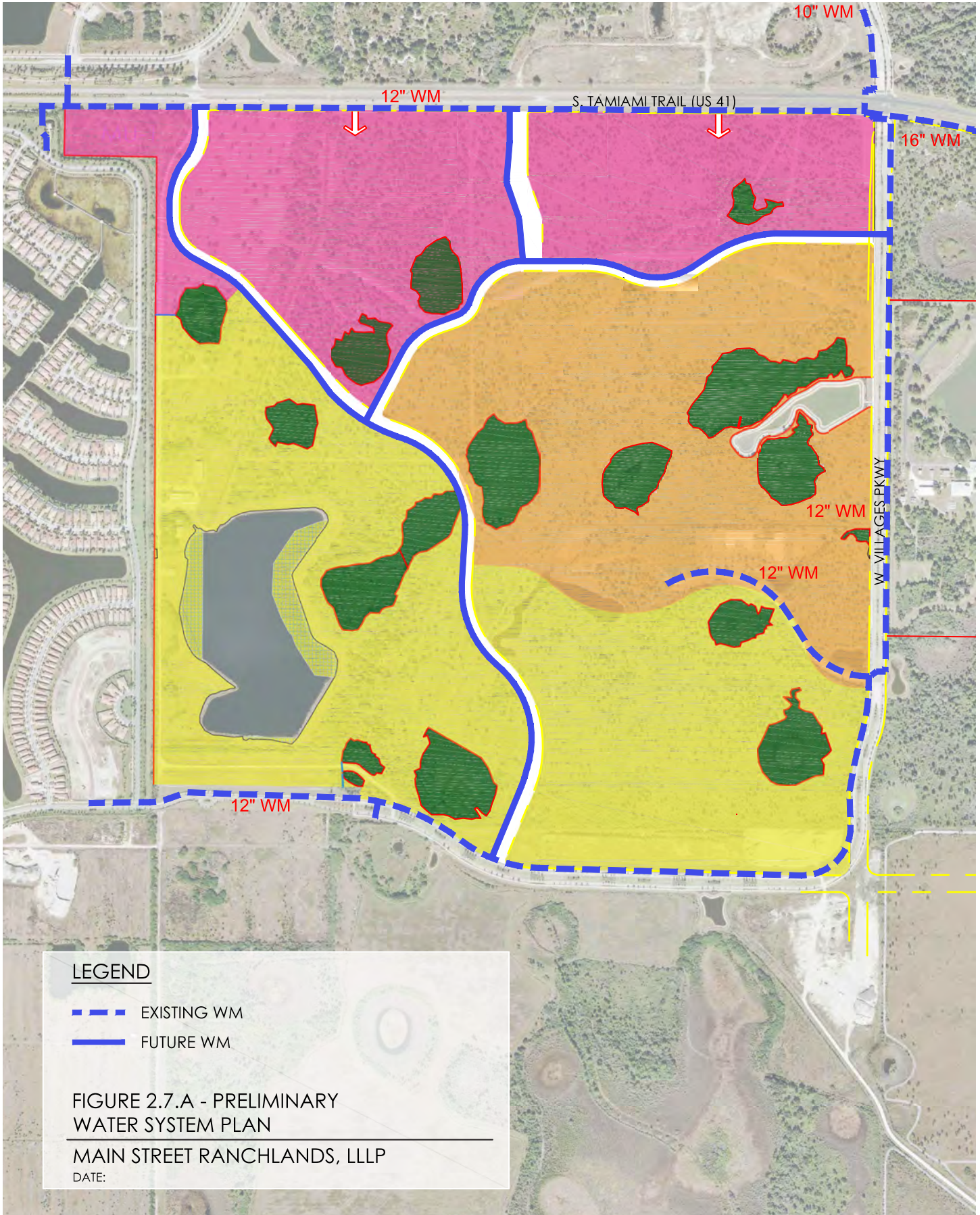
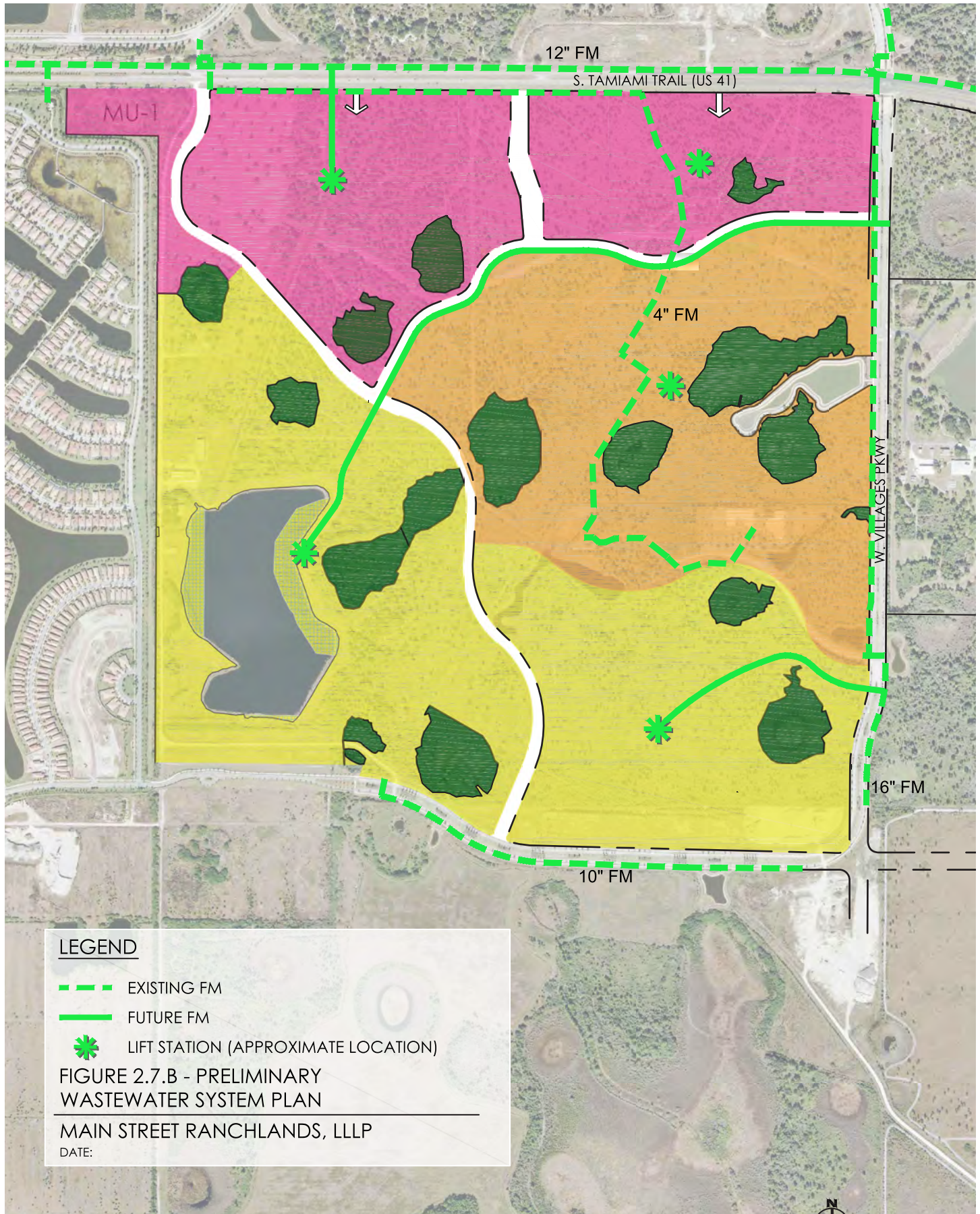


Figure 2.7.B.

Wastewater Systems Plan



Surrounding Character

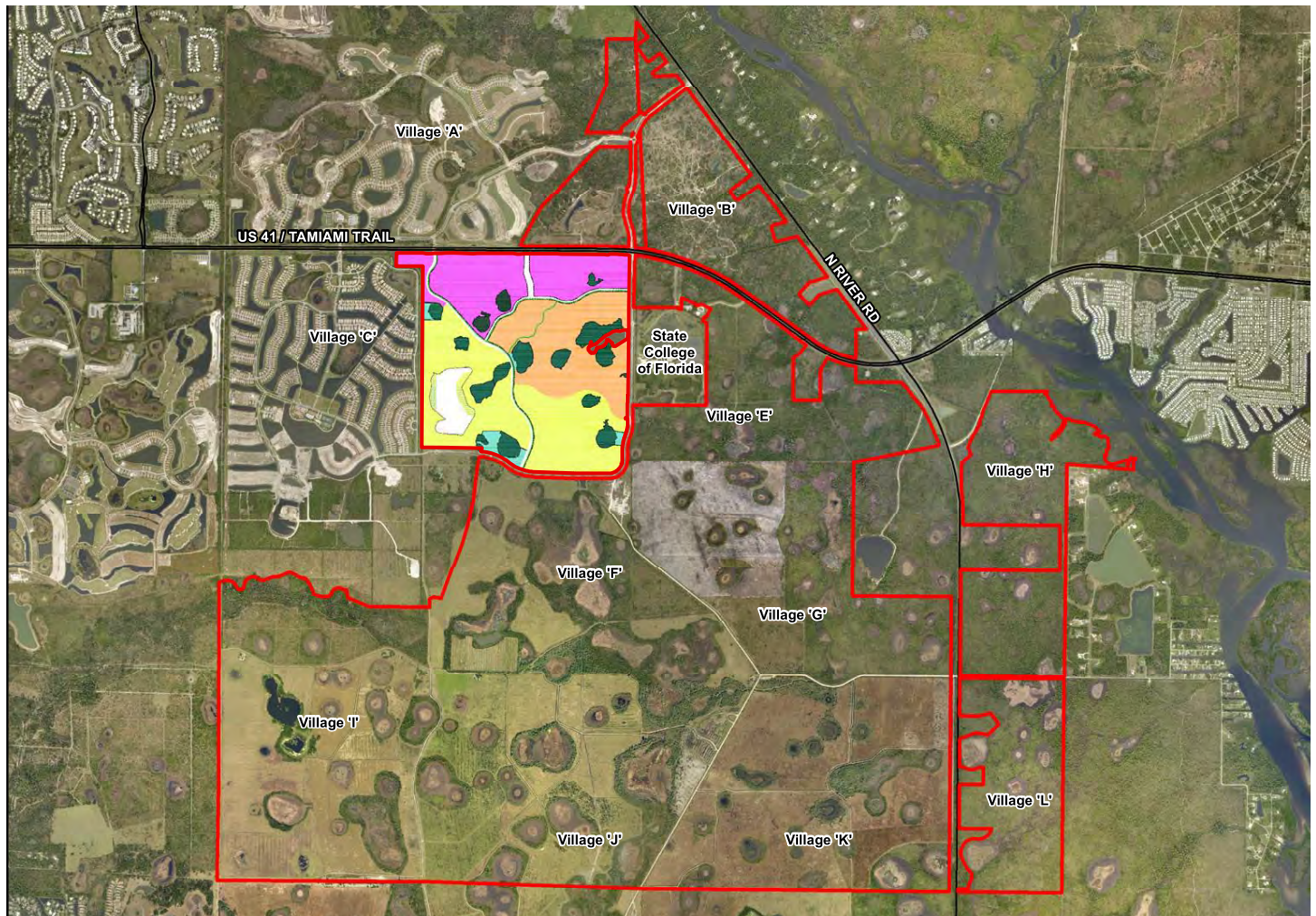
2.8 Surrounding Character

Village “D” is proposed within the West Villages area of the City of North Port. The West Villages has been conceptually planned to host a number of distinct villages, as well as a Town Center. Village “C” is located to the west of Village “D”, Village “A” is located to the north and Village “B” is located to the northeast. All three are in the development process. The Grand Palm Development of Critical Concern (DOCC) is located to the north, along River Road. The Villages of Manasota Beach DOCC is located to the southwest, south of Manasota Beach Road. Rural and semi-rural residential exists along the Myakka River.

interconnected roadway network, protecting and enhancing environmental assets, providing for the development of the Town Center and promoting a development pattern that will reduce reliance on the automobile.

The Preliminary VDPP design for Village “D” provides for a transition in density and intensity to ensure compatibility between nearby land uses. Furthermore, the preliminary design of Village “D” is intended to fulfill the planning guidelines of the West Villages by providing for an

Figure 2.8.A.
Surrounding Character



Definitions	
Residential Uses	
Community Center	Concentration of activities, services, and land uses that serve, and are focal points for, the immediate neighborhoods and used for recreational, social, educational, and cultural activities.
Gatehouse	Guardhouse and/or electronic barrier limiting access to a neighborhood.
Single-Family Detached	A building containing one dwelling unit and that is not attached to any other dwelling by any means and is surrounded by open space or yards.
Single-Family Semidetached	A one-family dwelling attached to one other one-family dwelling by a common vertical wall, with each dwelling located on a separate lot.
Townhouse Dwelling	A one-family dwelling in a row of at least three such units in which each unit has its own front and rear access to the outside, no unit is located over another unit, and each unit is separated from any other unit by one or more vertical common fire-resistant walls.
Stacked Townhouse Dwelling	Townhouses that have multiple units vertically, typically two in number, each unit having its own private entrance from the street or at least from the outside.
Multi-Family Dwelling	A building containing three or more dwelling units, including units that are located over another.
Mixed-Use Development	The development of a neighborhood, tract of land, building, or structure with a variety of complementary and integrated uses, such as, but not limited to, residential, office, manufacturing, retail, public, and recreation, in a compact urban form.
Accessory Apartment	An accessory dwelling unit with a separate means of ingress and egress containing separate kitchen, bathroom, and sleeping facilities, that is (1) physically attached to or contained within an existing single-family house or (2) occupies the second story of an existing garage or accessory building on the same lot as the principal dwelling.
Residential Support Uses	
Park/Recreation Facilities	A tract of land owned or controlled and used by government or specific and designated entities or persons for active and/or passive recreational purposes.
Active Recreation	Leisure-time activities, usually of a formal nature and often performed with others, requiring equipment and taking place at prescribed places, sites, or fields.
Passive Recreation	Activities that involve relatively inactive or less energetic activities, such as walking, sitting, picnicking, and board and table games.
Religious Use	A structure or place in which worship, ceremonies, rituals, and education pertaining to a particular system of beliefs are held.
Commercial/Service Uses	
Office	A room or group of rooms used for conducting the affairs of a business, profession, service, industry, or government and generally furnished with desks, tables, files, and communication equipment.
Services	Establishments primarily engaged in providing assistance, as opposed to products, to individuals, business, industry, government, and other enterprises.
Finance, Insurance, and Real Estate (FIRE)	Establishments such as banks and financial institutions, credit agencies, investment companies, brokers of and dealers in securities and commodities, security and commodity exchanges, insurance agents, lessors, lessees, buyers, sellers, agents, and developers of real estate (including drive through facilities).
Financial Service Center	a nonbank entity that does not accept deposits or make loans like traditional banks or financial institutions but that provides monetary services that include the sale or redemption of traveler's checks or money orders, money wire transfers, check cashing, and currency exchange
Assisted Living Facility	Residences for the frail elderly that provide rooms, meals, personal care, and supervision of self-administered medication. They may provide otherservices, such as recreational activities, financial services, and transportation.
Health-Care Facility	A facility or institution, whether public or private, principally engaged in providing services for health maintenance and the treatment of mental or physical conditions
Health-Care Services	Establishments providing support to medical professionals and their patients, such as medical and dental laboratories, blood banks, oxygen, and miscellaneous types of medical supplies and services.
Intermediate Care Facility (ICF)	A facility that provides health-related and personal care services to the chronically ill, disabled, or elderly people, including assistance with dressing and eating.
Health Club	An establishment that houses exercise equipment and space for the purposes of physical exercise.
Veterinary Hospital	A place where animals are given medical care and the boarding of animals is limited to short-term care incidental to the hospital use.
Artist's Studio	A place of work for an artist, artisan, or craftsman, including persons engaged in the application, teaching, or performance of fine arts such as, but not limited to, drawing, vocal or instrumental music, painting, sculpture, and writing.
Funeral Home	A building used for the preparation of the deceased for burial and the display of the deceased and rituals connected therewith before burial or cremation.
Research Laboratory	A facility for the investigation into the natural, physical, or social sciences, which may include engineering and product development
Research Park	A research facility or group of buildings dedicated to scientific research or business purposes, often linked to a major research facility.
Child-Care Center	An establishment providing for the care, supervision, and protection of children.

Retail Uses	
Commercial Use	Activity involving the sale of goods or services carried out for profit.
Retail Services	Establishments providing services or entertainment, as opposed to products, to the general public for personal or household use, including eating and drinking places; hotels and motels; finance, real estate, and insurance offices; personal services; theaters; amusement and recreation services; health, educational, and social services; museums; and galleries; self-storage (including drive through facilities).
Big-Box Store	A large, freestanding, single-story retail store, of generally more than fifty thousand square feet, and usually part of a regional or national chain (including drive through facilities).
Retail Store, Convenience	A retail establishment of up to five thousand square feet selling primarily food products, beverages, newspapers and magazines, candy, cigarettes, household items, and a limited amount of freshly prepared foods such as sandwiches and salads for off-premises consumption (including drive through facilities).
Gasoline Station and Convenience Center	A retail facility combining a gasoline station and a convenience store.
Retail Store, Express	Smaller version of the large-scale retail store (big-box retailer), serving rural areas or smaller urban areas.
Hotel/Motel	A facility offering transient lodging accommodations to the general public and that may include additional facilities and services, such as restaurants, meeting rooms, entertainment, personal services, and recreational facilities.
Theater	A building or part of a building used to show motion pictures or for drama, dance, musical or other stage performances.
Parking/Utility/Communication Uses	
Utility Box	Electric transformers switch boxes, telephone pedestals and telephone boxes, cable television boxes, traffic control boxes, and similar devices.
Parking Area	Any public or private area, under or outside of a building or structure, designed and used for parking motor vehicles, including parking lots, garages, private driveways, and legally designated areas of public streets.
Wireless Telecommunications Towers and Facilities	A parcel of land containing a tower, sending and receiving antennas attached to the tower, and a prefabricated or modular structure or cabinets containing electronic equipment
Essential Services	Services and utilities needed for the health, safety, and general welfare of the community, such as underground, surface, or overhead electrical, gas, telephone, steam, water, sewerage, and other utilities and the equipment and appurtenances necessary for such systems to furnish an adequate level of service for the area in which they are located, including community entry way, art, and monumentation.